

AGENDA

Lexington Planning Board

Thursday, January 30, 2025

Remote on Zoom: <https://www.lexingtonma.gov/377/Access-Virtual-Meetings>

6:00 PM

Development Administration

- 1. 231 Bedford Street - Public Hearing - Applicant request continuance**
Continued public hearing for a major site plan review for a multi-family development in the village overlay district (continued from 9/11 & 11/20). *Applicant requested continuance to March 5, 2025 Planning Board meeting.*
- 2. 185, 187-189 Bedford Street - Public Hearing**
Continued public hearing for a major site plan review proposal for a multi-family dwelling in the village overlay district. (Continued from 12/11)
- 3. 217, 229, 233, 241 Massachusetts Avenue - Public Hearing**
Continued public hearing for a major site plan review for a mixed-use, multi-family development in the village overlay district. (Continued from 9/25 and 11/20/24.)
- 4. 7 Hartwell Avenue - Public Hearing**
Public hearing for a major site plan review and special permit application for a multi-family development in the village high rise overlay district.

Board Administration

- 1. Board Discussion - Zoning Amendment Relative to Bicycle Parking**
Sec. 5.1.8: Request to postpone amendment relative to §5.1.8 bicycle parking.
- 2. Board Administration & Other Business Items**
Board Member & Staff Updates:
Review of Draft Meeting Minutes: 1/15/25
Review spring meeting schedule.
Upcoming Meetings: 2/12, 2/26, 3/5, 3/12, and Thur 3/27.

Adjourn

- 1. The meeting will continue until all items are finished. The estimated adjournment time is 10:30 pm**

Zoom Details

1. **Members of the public can attend the meeting from their computer or tablet by clicking on the following link at the time of the meeting**

Planning is inviting you to a scheduled Zoom meeting.

Topic: Planning Board Meeting

Time: Jan 30, 2025 06:00 PM Eastern Time (US and Canada)

Join Zoom Meeting

[https://lexingtonma.zoom.us/j/89157951555?](https://lexingtonma.zoom.us/j/89157951555?pwd=KmnyIYSPqAtb0E34uJTX5M0G1smPaF.1)

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Meeting ID: 891 5795 1555

Passcode: 701543

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AGENDA ITEM SUMMARY

LEXINGTON PLANNING BOARD

AGENDA ITEM TITLE:

231 Bedford Street - Public Hearing - Applicant request continuance

PRESENTER:

Staff

ITEM NUMBER:

SUMMARY:

The Planning Board will vote to further continue the public hearing without discussion on the application of 231 Bedford St LLC, for approval of a major site plan review under Zoning Bylaw §135-7.5 [Village & Multi-Family Overlay Districts] and §9.5 [Site Plan Review], and Stormwater Management review under Article VI of §181-71. Application is for a three-story residential building with 7 dwelling units with parking underneath, landscaping, and stormwater management improvements.

Application material may be viewed here (click files tab): <https://lexingtonma.portal.opengov.com/records/88897>

The Applicant has requested this hearing be further continued to the Board's next meeting on Wednesday, March 5, 2025 at or after 6:00 pm on Zoom without discussion.

SUGGESTED MOTION:

Move to accept the Applicant's request to continue the public hearing for the proposal at 231 Bedford Street to **Wednesday, March 5, 2025** at or after 6:00 pm on Zoom.

Move to accept the Applicant's request to extend the final action deadline to March 21, 2025.

FOLLOW-UP:

DATE AND APPROXIMATE TIME ON AGENDA:

1/30/2025

ATTACHMENTS:

Description

Type

▣ Extension Request

Cover Memo



Town of Lexington

PLANNING BOARD

1625 Massachusetts Avenue
Lexington, MA 02420
Tel (781) 698-4560

planning@lexingtonma.gov
www.lexingtonma.gov/planning

Michael Schanbacher, Chair
Robert Creech, Vice Chair
Melanie Thompson, Clerk
Charles Hornig, Member
Michael Leon, Associate Member

January 23, 2025

Lexington Planning Board

Re: Request for public hearing continuance and final action deadline extension

Project Address: 231 Bedford Street (PLAN-24-10)

To the Lexington Planning Board:

I am hereby requesting the continued public hearing scheduled for the major site plan review application submitted on May 14, 2024 by 231 Bedford Street, LLC agrees to further continue the public hearing to the Planning Board's March 5, 2025 meeting. Further, the applicant agrees to extend the final action deadlines as follows to allow more time to submit requested materials (primarily peer review consultant fees to complete the stormwater review and base flood elevation analysis).

Current meeting date: January 30, 2025 to March 5, 2025

Extension Decision deadline date: from March 21, 2025

Respectfully,

Signature of Applicant or Applicant's representative

231 Bedford Street LLC

AGENDA ITEM SUMMARY

LEXINGTON PLANNING BOARD

AGENDA ITEM TITLE:

185, 187-189 Bedford Street - Public Hearing

PRESENTER:

Applicant: 185 Bedford Street LLC

ITEM NUMBER:

SUMMARY:

The Planning Board will re-open the continued public hearing on the application of 185 Bedford Street LLC, for review of a major site plan review application submitted under §135-7.5 [Village & Multi-Family Overlay Districts] and §9.5 [Site Plan Review] of the Zoning Bylaw, and pursuant to Article VI of §181-71 for Stormwater Management Regulations. Proposal also includes a special permit, pursuant to 5.1.14 and 5.1 [Off-Street Parking and Loading] for relief of the driveways to be set back less than 5 ft. from the wall of the principal building (5.1.11.3). The application is for a three-story multi-family residential dwelling containing 25 dwelling units, 27 parking spaces under the building, landscaping, and stormwater management improvements.

Application materials may be viewed (click the files tab): <https://lexingtonma.portal.opengov.com/records/97104> (new files uploaded after 1/9)

Applicant will present update since the last meeting, staff and peer review consultant will provide a report, and board members will discuss. The Chair will then re-open the hearing up for public comments. After public comments the board may discuss further and the applicant may respond to comments.

Staff and peer review memo are attached.

At the end of the discussion, the Board may vote to continue the hearing to a future meeting date or vote on the application. Staff believes revised material submitted meets all the Zoning Bylaw and the Board's Regulations and recommends approval. Staff has prepared a draft approval decision for the Board's review.

SUGGESTED MOTION:

Move to continue the public hearing to Wednesday, February 12 on Zoom at 6:00 pm.

Or, if the Board is ready to vote on the application below are suggested motions (may change during the course of the meeting).

Move to close the public hearing for the major site plan review and special permit application for 185, 187-189 Bedford Street.

Waivers from the Board's Regulations:

1 - Move to waive the following strict requirements of §176-12.9.5 because the Board finds the project meets the intent of and purposes of Board’s Regulations.

- a. Section 4 preference for country drainage.
- b. Section 5 incorporating low-impact development techniques.
- c. Section 6 for permeable pavement.
- d. Section 8 requiring the use of stormwater harvesting systems, such as cisterns and ponds for plant irrigation.

Special Permit:

Move to grant 3 ft. of relief to allow the driveways to be within 5 feet of the wall of the principal building.

Site Plan Review & Stormwater Permit Application:

Move to approve the proposal submitted by 185 Bedford Street LLC with the findings and conditions included in the draft approval decision prepared by staff for major site plan review with stormwater permit and the 52 conditions as may be modified this evening.

Move to have the Chair sign the decision and correct any non-substantive changes such as grammar, typos, and for consistency.

FOLLOW-UP:

DATE AND APPROXIMATE TIME ON AGENDA:

1/30/2025

ATTACHMENTS:

Description	Type
<input type="checkbox"/> Staff Memo 1.23.25	Cover Memo
<input type="checkbox"/> Peer Review Eaglebrook Memo 1.24.25	Cover Memo
<input type="checkbox"/> Peer Review Eaglebrook Memo 1.22.25	Cover Memo



TOWN OF LEXINGTON
PLANNING OFFICE

1625 Massachusetts Avenue
Lexington, Massachusetts 02420
Tel: 781-698-4560

planning@lexingtonma.gov
www.lexingtonma.gov/planning

Abby McCabe, Planning Director
Meghan McNamara, Assistant
Director
Carolyn Morrison, Planning
Coordinator



To: Lexington Planning Board

From: Meghan McNamara, Assistant Planning Director

**Re: Site Plan Review for 185 & 187-189 Bedford Street; Village and Multi-Family Overlay District;
Staff Memo #2**

Date: January 23, 2025

The Applicant submitted the following material for the public hearing on January 30, 2025: architectural planset (rev. 1/3/25), civil planset (rev. 1/9/25), peer review response memo (dated 1/8/25), stormwater mgmt. report (rev. 1/8/25), water and sewer flows narratives, and a draft easement agreement for parking at 186 Bedford St.

No response to the first staff memo was provided as of January 23.

A second memo from peer review consultant Michael Juliano of Eaglebrook Engineering & Survey, LLC, dated 1/22/25 is provided as a separate letter.

Project Summary:

The proposed development includes a three-story, 37,169-square foot residential dwelling containing 25 dwelling units and a two-level garage with 27 parking spaces. The dwelling unit mix includes 1-bedroom units with and without a study and 2-, and 3-bedroom units ranging in size from ~577 – 1269 square feet. Amenities for the residential use include a lobby area with mailboxes, 38 long-term, indoor bicycle parking spaces and outdoor passive recreation areas. There will be two (2) driveways permitting access to and from the site off of Bedford Street. The driveways are located on opposite sides of the building and each leads to a separate level of the parking garage. A total of three (3) Inclusionary Dwelling Units (IDU) are proposed with household income limited to 80% of the Area Median Income.

The two levels of the garage have been redesigned with frosted glazing on the windows. We appreciate this revision as it is still architecturally pleasing and considerate of the abutting properties. Space for compost is allocated in the gated trash/recycling area. Applicant has provided turning diagrams for the following scenerios: a delivery truck entering and exiting the north driveway in a forward moving direction, and a waste hauler vehicle entering and exiting the south driveway in a forward moving direction.

Staff Comments:

At the previous Planning Board meeting, potential shared parking opportunities with the property across the street was discussed. It was requested that the Applicant provide shared parking option for overflow via an easement. No new parking information has been submitted. Building Commissioner has reviewed the Average Natural Grade (ANG) and proposed building height and confirms that the height to the top of

the ridge from ANG is 40 feet. 40 feet is allowed in the VO District and the number of stories is not restricted, so this complies. Proposed height will be reviewed again for final compliance prior to issuance of a building permit. Applicant proposes a combination of Sweetspire, Red Maples, and Swamp Azalea along the rear property boundary where a landscaped transition area is required because it is the boundary of the VO District.

Proposed Unit Count:

(x/x) = (Market-Rate Total/IDU Total)

Unit-Type	Total Number	Bedroom Count	% of Unit Total
1 Bedroom	10 (9/1)	10	40%
1 Bedroom + study	4 (4/0)	4	16%
2 Bedroom	9 (7/2)	18	36%
3 Bedroom	2 (2/0)	6	8%

-Unit sizes range from 577 SF to 1,269 SF

-3 Inclusionary Dwelling Units (IDU) are designated to households earning 80% of the Area Median Income (AMI).

-One two-bedroom IDU is also proposed as Group 2 Accessible.

Vehicle and Bicycle Parking:

Parking Type	Amount Required	Amount Proposed	Notes
Long-Term Bike Storage	1.5 per unit	38	In Compliance
Short-Term Bike Storage	0.1 per unit	8	In Compliance
Vehicle Parking	1 per unit	27	In Compliance
EV Charging ^{1,2}	Min. 4%	2	In Compliance
Compact Vehicle Spaces	Max. 33%	6	In Compliance

¹ - For the purposes of this requirement, both levels of the garage are being considered as one parking lot.

² - Per 135-5.1.13(11) the parking lot [garage] shall be constructed with appropriate conduits and space for transformers and switchgear to allow for future installation of EV charging stations for a minimum 50% of total off-street parking spaces.

Special Permits Needed:

A special permit pursuant to §5.1.14 is required for relief to allow the driveway to be within 5-ft. of the wall of the building §5.1.13.1.(d), relief of 3 ft. needed to provide 18 ft. wide driveway.

135-5.4.4.2 – all luminaires, regardless of lumen rating, shall be equipped with whatever additional shielding, lenses, or cutoff devices are required to eliminate light trespass onto any abutting street. Revise plans.

Waivers Needed:

176-12.4.5.1 – Pedestrian ways shall be at least four feet in width.

Outstanding Items:

Chapter 135 Zoning Bylaw Review

- Section 5.1.13.11 requires that a parking lot with 25 or more parking spaces shall be constructed with appropriate conduits and equipment to allow for future EV for 50% of the spaces, or 14 spaces. Show on plans where the future EV spaces are proposed, understanding that the location may change during more detailed design process. The Fire Department requests the majority of EV parking spaces be located on the same level and in close proximity to the garage entrance/exit.
- No preliminary signage package was submitted. Site renderings show the numbers “185” and “189” mounted on the front wall of the building. The Board can condition that final signage be reviewed and approved during the building permit stage.
- Please provide on a plan, the setback distance to the furthest projecting points of the proposed new building on civil plan. If this information is shown, please direct staff to this plan. Thank you.

Remains outstanding

Setback	Required (VO District)	Proposed	Comments
Front Yard	15 feet	20 feet	See note above
Side Yard	15 feet	15.6 feet	See note above
Rear Yard	15 feet	20.5 feet	See note above

Chapter 176 Planning Board Zoning Regulations Review

- Notes on the Construction Management Plan reference Piper Road, please revise ***Remains outstanding***
- Will any earth be imported, exported and/or regraded on site? Include calculations detailing the amount on the Construction Management Plan if so. ***Remains outstanding***
- Show location of construction staging on construction plans. ***Remains outstanding***
- Which lighting will remain on between 11 pm and 6 am? ***Remains outstanding***
- 12.4 - “Pedestrian ways shall be at least four feet in width”. Plans show a 3-foot wide stone dust walkway from Bedford Street around the building to the rear. (Building Commissioner notes a minimum 4-foot width is required for accessibility).
- 12.6 - Recommend larger tree wells or at ground level for the two Acer Rubrum in front of the building to provide sufficient room to support hearty growth of these large shade trees. ***Plans remain unchanged, no response***
- 12.8 - There is minimal light spillage onto the Bedford Street right of way near the driveways. Please adjust so there is no light spill over the property lines.
- 12.8 - Provide spec sheets for proposed outdoor lighting to confirm compliance with Zoning Bylaw and Planning Board’s Zoning Regulations. ***Remains outstanding***
- 12.9 - Section 12.9.5.3 says “all basement floors and slabs shall be at least two feet above the estimated seasonal high groundwater table”. The Site Analysis Plan shows two test pit locations on the southern edge of the property. Please include the results of these test pits on the Site Analysis Plan as well. ***Remains outstanding***
- Section 12.9.5.8 says “use stormwater harvesting systems, such as cisterns and ponds, for plant irrigation”. The Applicant should explore ways to meet this requirement, or request a waiver and provide waiver justification. ***No response***

Planning Staff Comments

- Is “Patriot Way” a potential name for the development? Recommend against this as Patriot Way is already a street in Lexington on the opposite side of town **No response**
- Is there an anticipated start date for construction? Applicant notes 12-15 months for anticipated construction timeline. **No response**
- Show location of construction fencing along the frontage of Bedford Street. This is to prevent width of the curb cut to be used as a construction entrance and for the safety of pedestrians. **No response**
- Is any sidewalk closure anticipated? What is the temporary detour plan if so. **No response**
- Request signage installed at both exits warning drivers to watch for bicycles and pedestrians. **No response**
- All retaining walls shall be set back from the property line the height of the structure. **Show TOW and BOW elevations for the loading area retaining wall**
- Accessibility – [521 CMR 9.4](#) requires that 5% (2 units) of the dwelling units shall be designed as Group 2A Dwelling units that comply with CMR 9.5 for bathrooms, kitchens, and bedrooms. **Applicant proposes two Group 2A dwelling units, this complies.**
- Applicant should verify mailbox locations with the Post Master. **No response**
- What is the proposed height of the garage entrance doors? 98” of vertical access is required for the accesssible space, aisle and vehicular route. **No response**
- How does the Applicant intend to protect air quality during construction? And promote sustainability awareness and education? Both are criteria selected on the SITES checklist. **No response**
- Please explain the trash/recycling removal plan. There is an internal trash room on the ground floor in addition to external dumpster area. Will this be private removal? What can be done to prevent the permiter gate to the dumpster from swinging open and potentially hitting vehicles? **No response**
- Please explain how the e-bike charging will work in the storage room. Can multiple bikes charge at once? **No response**
- Has the Applicant considered unbundling the cost of parking from rental units? This would encourage fewer cars on the road and lower living expenses.

Fire Department Comments

- Confirm that the infiltration systems can support the weight of Lexington’s heaviest fire truck apparatus. DRT response states that this has been provided but where.
- Show the centerline of Bedford Street on the fire truck turning diagrams.
- Can the length of a 250-foot fire house connect around the entire building from either side? If so, the requirement for a fire truck access path may not be necessary.
- Request that the two EV Car Charging Stations be located on the same garage level and in close proximity to the entrance/exit door.
- Where is the sprinkler room located? This shall have an exterior access door
- The bike storage room, since providing electric bike charging, shall have an exterior access door and be fire-rated. No access from the building interior is permitted.

Engineering Department Comments

- Applicant has provided proposed water and sewer flow calculations to the Engineering Department for further review. Applicant may be required to perform a downstream sewer and water capacity analysis for site utilities if required. Applicant is responsible for the analysis and any required improvements.

Building Department Comments

- The State Energy code requires that 20 % of all parking be EV spaces, of which, should one be ADA/Accessible EV space ?, one of which could be Van Accessible, the code is not clear
- The accessible parking accessible access aisle is not dimensioned, min width required for a van is 8 feet
- Will there be a need for a guardrail at the garage entry drive on left side, looks like there's a 7 foot retaining wall at that corner.
- There are 2 units designed as Group 2 Accessible, All other units must meet Group 1 per Mass Building Code , Architectural Access Board
- Ensure outdoor amenities area , kitchen, BBQ, walkways, benches, picnic tables, playground equipment and all surfaces are ADA/AAB compliant
- All retaining walls must be set back a distance equal to the height of the wall
- Are there any fences planned to be installed?
- Will you be using the 10th edition of the State Building Code? Its effective for permit applications starting July 1, 2025.
- Would the applicant be amenable to pay for a third party consultant to assist the Building Inspectors for plan review of ADA/AAB/FHA code requirements, and to perform Inspections?
- Will there be a direct entry from the exterior for access to fire sprinkler roof and Main Electrical room, with signage per code, for emergency response from fire department
- The State Architectural Access board has determined that stone dust is not a code compliant sidewalk surface for persons with a Disability, please consider a paved or concrete side walk, min. 4 feet wide
- Will exit/egress side walk from rear to front be graded to be less than 5 %?
- The buildings first floor second means of emergency egress cannot pass into and through garage
- One of the Accessible units is also an inclusionary unit, this should be discussed with Housing specialists
- Review Accessible paths to recycle and dumpster, if the access to the exterior dumpster is for tenants, the route and gates must be accessible
- Does building cantilever over driveway? If so, confirm clearances for vehicles



Eaglebrook Engineering & Survey, LLC

Civil Engineers, Land Planners and Land Surveyors

January 24, 2025

Lexington Planning Board
Attn: Ms. Abigail McCabe, AICP
Town of Lexington
1625 Massachusetts Avenue
Lexington, Massachusetts 02420

**RE: 185-189 Bedford Street
Lexington, MA.
Second Review Letter**

Dear Members of the Planning Board:

Eaglebrook Engineering & Survey, LLC (EES) has been retained to review the above referenced project for an application for Major Site Plan Review with Stormwater Permit. The scope of EES's review is the Planning Board Zoning Regulations Chapter 176 Compliance-Section 12.9 Utilities and Stormwater Management Compliance -Chapter 181 Article VI.

The following documents were reviewed by EES:

- Site Plan Review Plan Set dated October 5, 2024, Revised January 24, 2025, prepared by Patriot Engineering, (19 sheets).
- Stormwater Management Report and Calculations for a Multi Family Development dated October 5, 2024, Revised January 22, 2025, prepared by Patriot Engineering.

The applicant has addressed the outstanding stormwater issues identified in our review letter dated January 22, 2025. The planning board should be aware of the waiver requests requested by the applicant.

We look forward to discussing the project at the January public hearing.

Sincerely,
EAGLEBROOK ENGINEERING & SURVEY, LLC

Michael J. Juliano, P.E., P.L.S.
Principal



Eaglebrook Engineering & Survey, LLC

Civil Engineers, Land Planners and Land Surveyors

January 22, 2025

Lexington Planning Board
Attn: Ms. Abigail McCabe, AICP
Town of Lexington
1625 Massachusetts Avenue
Lexington, Massachusetts 02420

**RE: 185-189 Bedford Street
Lexington, MA.
Second Review Letter**

Dear Members of the Planning Board:

Eaglebrook Engineering & Survey, LLC (EES) has been retained to review the above referenced project for an application for Major Site Plan Review with Stormwater Permit. The scope of EES's review is the Planning Board Zoning Regulations Chapter 176 Compliance-Section 12.9 Utilities and Stormwater Management Compliance -Chapter 181 Article VI.

The following documents were reviewed by EES:

- Site Plan Review Plan Set dated October 5, 2024, Revised January 9, 2025, prepared by Patriot Engineering, (19 sheets).
- Stormwater Management Report and Calculations for a Multi Family Development dated October 5, 2024, Revised January 8, 2025, prepared by Patriot Engineering.
- Peer Review Response letter dated January 8, 2025, prepared by Patriot Engineering.

Planning Board Review-Section 12.9

12.9.1 and 12.9.2: The applicant has provided estimated sewage flows based on Title 5 and the calculations are acceptable.

12.9.4: The planning board should review the locations and screening of the meters and other infrastructure elements to ensure that the applicant has met the board's requirements.

12.9.5.6: The project does not provide permeable pavement or other means to reduce impervious surfaces.



Eaglebrook Engineering & Survey, LLC

Civil Engineers, Land Planners and Land Surveyors

12.9.5.8: The project is not proposing any harvesting systems to collect rain to be used for the irrigation of the landscape areas. The applicant is requesting a waiver.

Stormwater Management Compliance -Chapter 181 Article VI

Stormwater Analysis

- The existing and proposed HydroCAD calculations were not changed to HSG A. The calculations are based on HSG D.
- The stormwater report on sheet 2 indicates that there were no signs of groundwater but soil log on the plan sheet C300 indicated groundwater was encountered.
- The applicant is requesting a waiver to design low impact development techniques.

Site Development Plans

- The Applicant should review the revised fire and water service connections design with the Lexington DPW.
- The applicant has added an overflow from proposed subsurface infiltration system PSIS-2 to avoid overflow to the trench drain and the applicant added an overflow from PSIS-1. The overflow pipes will be discharged to vegetated depressions. The depressions are located at proposed locations of red maple trees. The landscape plan should be revised to reflect the locations of the two depressions and should provide landscape details for the vegetation within the depressions.
- The applicant has re-located the retaining wall further away from PSIS-1. EES would recommend the placement of an impervious barrier along the wall to prevent stormwater from discharging through the wall. The wall will need to be designed for hydrostatic loading due to the stormwater infiltration system.

We look forward to discussing the project at the January public hearing.

Sincerely,

EAGLEBROOK ENGINEERING & SURVEY, LLC

Michael J. Juliano, P.E., P.L.S.
Principal



Eaglebrook Engineering & Survey, LLC

Civil Engineers, Land Planners and Land Surveyors

AGENDA ITEM SUMMARY

LEXINGTON PLANNING BOARD

AGENDA ITEM TITLE:

217, 229, 233, 241 Massachusetts Avenue - Public Hearing

PRESENTER:

Applicant: North Shore Residential
Development

ITEM NUMBER:

SUMMARY:

The Planning Board will re-open the public hearing on the application of North Shore Residential Development, Inc., for approval of a major site plan review under §135-7.5 [Village & Multi-Family Overlay Districts] and 9.5 [Site Plan Review] of the Zoning Bylaw and Article VI of §181-71 Stormwater Management Regulations. Proposal also request a special permit, pursuant to Section 5.1.14 and 5.1 [Off-Street Parking and Loading]. Application is to construct a 44-unit five-story mixed-use building with first floor commercial parking spaces, landscaping, and stormwater management improvements.

Application materials may be viewed here (click Files tab): <https://lexingtonma.portal.opengov.com/records/94025> (*new material uploaded Dec. 5 & after*)

The Applicant will present update since the last meeting, staff and peer review consultant will provide a report, and board members will discuss. The Chair will then re-open the hearing up for public comments. After public comments the board may discuss further and the applicant may respond to comments. At the end of the night the Board will vote to continue the hearing to a future meeting date.

Staff memo and peer review consultant memo attached.

SUGGESTED MOTION:

Suggested motions at the end of the discussion.

Move to continue the public hearing for the site plan review and special permit application at 217, 229, 233 & 241 Massachusetts Avenue to Wednesday, March 5 to allow the applicant more time to respond to peer review comments, staff comments, and advance the proposal with the Conservation Commission.

Ask the Applicant to request extension of final action deadline.

Move to accept the applicant's request to extend the final action deadline to ____.

FOLLOW-UP:

DATE AND APPROXIMATE TIME ON AGENDA:

1/30/2025

ATTACHMENTS:

Description	Type
☐ Peer Review Memo 217-241 Mass Ave	Cover Memo
☐ Staff Memo	Cover Memo

January 23, 2025

Abby McCabe, Planning Director
Town of Lexington
Planning Office
1625 Massachusetts Avenue
Lexington, MA 02420

RE: 217-241 Massachusetts Avenue, Lexington, Massachusetts, Planning Board Site Plan & Wetland Protection Notice of Intent Peer Review.

Dear Ms. McCabe:

GCG Associates, Inc. has reviewed the following information for the Site Plan Review for 231 Massachusetts Avenue mixed use multi-family residential and commercial retail development project in Lexington, MA.

Planning Board Site Plan Review Submittal:

Documents:

1. Stormwater Report, 5 Story Mixed Use Development 217-241 Massachusetts Avenue, Lexington, Massachusetts, prepared by Sullivan Engineering Group, LLC (SEG). dated August 22, 2024, last revised January 9, 2025.
2. Response to peer review letter – 217-241 Massachusetts Avenue, prepared by SEG, dated January 9, 2025.
3. Construction Management Plan & Schedule, submitted by North Shore Residential Development, Inc., dated August 7, 2024, last revised January 6, 2025

Plan:

1. 231 Massachusetts Avenue, Lexington, MA 02420, Village overlay Development Project, Mixed-Use Retail & Residential Building – Business & Residential (OCC. Groups B & R-2). Site plan set consists of Title Sheet & Drawing List (plan sheets T-100 to T-101), dated 08/12/2024, last revised 10/30/2024; Civil Plan Set (sheets C-1 to C8), prepared by Sullivan Engineering Group, LLC. (SEG), dated 08/12/2024, last revised 10/30/2024; Landscape Architectural Plan Set (sheets L-R, dated 10/28/2024, L-1 to L-9), prepared by J. Thomas Land Design Studios, dated 10/30/2024; And Architecture Plan Set (sheets A-100 to A-106, sheets A-201 to A-204, and sheets A-301 to A-303), prepared by Scott Melching Architect LLC. dated 10/30/2024., 35 plan sheets total.
2. Riverfront Mitigation Plan, Tax Map 13 Lot 383, Arlington Owned Land within Lexington, Mass. Prepared by SEG, dated 1/2/2025.

Conservation Commission Notice of Intent

Documents:

1. Amended Notice of Intent (NOI) for DEP File #201-1338, prepared by Norse Environmental Services, Inc., (NES), dated October 2024, last revised January 14, 2025.
2. Request to amend NOI letter, prepared by NES, dated January 14, 2025
3. Riverfront Area Development Alternative Analysis, prepared by NES, dated January 14, 2025.
4. Stormwater Report, 5 Story Mixed Use Development 217-241 Massachusetts Avenue, Lexington, Massachusetts, prepared by Sullivan Engineering Group, LLC (SEG). dated August 22, 2024, last revised October 30, 2024. (Identical to the PB site plan submittal)

Plan:

1. Residential New Construction, 231 Massachusetts Avenue, Lexington, MA 02420, Civil plan set consists of 8 sheets (C-1 to C8), prepared by Sullivan Engineering Group, LLC. (SEG), dated 08/12/2024, last revised 01/09/2025. (Identical to the PB submittal site plan Civil sheets).
2. Residential New Construction, 231 Massachusetts Avenue, Lexington, MA 02420, Landscape Architectural Plan Set consists of 10 sheets, (sheets R-1, L-1 to L-9), prepared by J. Thomas Land Design Studios, dated 10/30/2024, last revised 01/09/2025. (Identical to the PB submittal site plan Landscape Architecture sheets).
3. Riverfront Mitigation Plan, Tax Map 13 Lot 383, Arlington Owned Land within Lexington, Mass. Prepared by SEG, dated 1/2/2025. (Identical to the PB submittal Riverfront Mitigation plan).

Based upon our review of the above Plan, we offer the following comments with respect to compliance with Chapter 181, Article VI. - Stormwater Management Regulations, Section 176-12.0, Section 13.9 Utilities. And The Lexington Conservation Commission Pursuant to the Code of the Town of Lexington for Wetland Protection, Chapter 130, Section 5 - Performance Standards. This peer review is limited to the Site Utilities and Drainage Mitigation compliance only. The numerical section of the regulations are referenced at the beginning of each comment unless it is a general comment. GCG latest comment shown on “Red.”

Site Visit: 09/18/2024, 11:00 AM - Planning Board members. Planning Department, Development team - Architects, Project Engineer, property owner, and Developer. GCG peer review engineer.

GCG witnessed SEG deep hole soil testing on 9/25/2024.

General Comments:

The project site consists of approximately 30,886 square feet (0.71+/- acres) of developed land. The northeast portion of the site (approximately 40+/- % of the parcel area) is located within Mill Brook’s 200 feet Riverfront (wetland resource) Area, which is protected under the M.G.L. Chapter 131 Section 40 and 310 CMR 10.00 Wetlands Protection and Town of Lexington Chapter 130. Therefore, this project requires filing a Wetland Notice of Intent with the Lexington Conservation Commission and MassDEP. Mill Brook (MA 71-07) is listed as impaired water with the Total Maximum Daily Loads, TMDLs restrictions.

1. The Civil plan set did not specify the plan scale. The plan set appeared to be at 1” = 20’ scale. GCG recommends showing the plan scale with a scale bar on all plan sheets, where applicable. [Resolved.](#)

Existing Conditions Plan (C-1)

271-241 Massachusetts Avenue, Lexington, MA

2. The Soil Test Pit - TH-1 information shown on this plan indicated an estimated seasonal high ground water (E.S.H.G.W.) at elevation 161.0. However, the GEI Geotechnical Engineering Report, Section 4.2, which recorded a 4/23/2024 monitoring well 'B1(MW)' water level reading at 164.6. The monitoring wells reading varied between 161.5 to 164.6, which were all higher than elevation 161.0 from TH-1. This plan shows the Mill Brook's bank identified by wetland flags A4 to A7 by Norse Environmental Services in August of 2024. As part of the Wetland Notice of Intent filing, a wetland report should be provided to identify any Bordering Vegetated Wetland (BVW) associated with the Mill Brook, based on the BVW Delineated Handbook (Second Edition, September 2022) and Bordering Vegetated Wetland Determination Form (delineated by wetland vegetation, Hydric Soil, and wetlands hydrology indications) which will provide additional indication of the seasonal high ground water elevation. The applicant should clarify how the E.S.H.G.W. was determined in TH-1., (by mottling, standing water, and/or weeping). Test pit TH-1 and the monitoring well B3(MW). The measured water level was at elevation 161.5 and were both dated 5/28/2024. The two locations are within 50 feet apart. Therefore, the ESHGW at TH-1 needs additional clarification. GCG recommends comparing the highest monitoring well reading to the nearby regional USGS monitoring well data and adjusting the reading by the USGS (Frimpter) method, per Massachusetts Stormwater Handbook (MSH) Vol. 3. Ch.1, Pg.12 - "Determine Seasonal High Groundwater" requirements. The proposed drainage field #1 is located over existing grades between 165.6 to 175.5 and the proposed drainage field #2 is located over existing grade between 165.7 to 174+/-, GCG recommends performing additional soil test pits at the proposed infiltration system locations at various elevations. [GCG concurs with the ESHGW elevations established through the on-site deep hole test pits.](#) Resolved
3. The GEI report indicated silty sand was found in all boring logs (GE1 to GEI-5, 4/23/2024 to 4/25/2024) and stated Grain Size Tests performed for GEI-1, GEI-2, GEI-4, and GEI-5 samples. Grain Size Test reports should be provided to evaluate the proper Rawls value for exfiltration rate, using the upper or lower value for the HSG 'A' site soil. [GCG concurs with using Loamy Sand and an infiltration rate of 2.41 in/hr for infiltration design. Any variation in the soil conditions discovered during construction should be reported to the Planning Department.](#) **Statement.**

Site Plan (C-2)

4. The site layout plan indicated impervious area alternation within the outer (between 100' and 200' of the Mill Brook Riverfront Area) riparian area. Based on the Landscape plan sheet L-1, referenced on the site plan, portion of the existing lawn area has been replaced with porous paver and playground safety surface. (Playground safety surface details should be provided, assumed to be permeable and designed accordingly). The proposed impervious coverage (roof and pavement surfaces) appeared to be extended further toward Mill Brook than existing. Any alteration within the 200 feet Riverfront Area should require Conservation Commission approval through an Order of Conditions. Under 310 CMR 10.58 (4), an alternative analysis should be included in the wetland Notice of Intent filing. The filing should identify BVW boundary (if any) associated with Mill Brook, provides detailed square footage of alternation within the Riverfront Area. 310 CMR 10.58 (5)(g) alternation within the Riverfront Area notwithstanding the criteria of 310 CMR 10.58(5)(c), (d), or (e) should be mitigated at a ratio in square feet of at least 2:1. [See Conservation Commission Notice of Intent comments below.](#)

5. Plans C-2 and L-1 showed a proposed handicap ramp and porous paver amenity area within the Minuteman Bikeway right-of-way (ROW), which is outside the project property and study watershed boundary. This off-site area is within the Riverfront Area and under the wetland protection/Conservation Commission's authority and should be included in the stormwater analysis. The proposed handicap ramp appeared to be reinforced concrete (impervious) materials, and the porous paver area is an alternation to natural vegetation and should require mitigation. This ROW is owned by the Massachusetts Bay Transportation Authority, MBTA and leased to the Town of Lexington, any works proposed within the ROW and the 30 feet Zone-of-Influence (ZOI) would require MBTA and the Town of Lexington approval, the operation and maintenance of the off-site improvements and responsible party should be addressed. (See additional porous paver/porous pavement comments below). **The applicant will submit a separate Notice of Intent for any work proposed off-site. An amended NOI has been submitted to include Tax Map 13 Lot 383 improvements to mitigate the Riverfront Area alternation, see additional comments under Conservation Commission NOI comments below. The proposed concrete pavement (exterior bike racks) in front of the first-floor bike parking room does not match the Civil plan sheet C-2, which called for pavers (permeable). Since this area is part of the watershed 3S, GCG recommends replacing the concrete pavement with permeable pavers surface.**

Grading, Drainage and Utility Plan (C-3)

6. 181-73. B.(2)(e) – Water velocity shall not be more than (5) five feet per second on paved surface. The proposed main driveway access to the subsurface garage consists of 10% slope. (The applicant should provide calculations to show driveway runoff flow velocity and control velocity to below 5 feet per second. **The runoff velocity over paved surface depends on the design flow rate and pavement longitudinal slope and cross slope. GCG recommends providing gutter flow calculations to prove the flow velocity meets or below the 5 feet per second threshold or provide gutter drains to control the runoff velocity. The applicant has requested a waiver for section 181-73. B.(2)(e). GCG recommends performing the flow velocity calculations based on the Manning's equation and Rational Method to estimate the peak runoff. Based on the small watershed area, the pavement gutter flow velocity should not exceed the five feet per second maximum velocity.**
7. The proposed 2-foot stone base underneath the Fire Lane (located at the westerly lot corner) was designed as an infiltration system and modeled as such in the HydroCAD calculations. The MSH's Table RR (Vol. 1, Ch.1 Pg.8) requires infiltration BMPs be setback 10 feet from property line and building foundations. **Resolved.**
8. The proposed infiltration system (Drainage Field #1 & #2) consists of commercially manufactured stormwater infiltration devices (Cultec Recharger 280HD Chambers), the system is classified as Shallow UIC Class V Injection Wells and should comply with the MassDEP's EEA, Energy and Environmental Affairs' Standard Design Requirements for Shallow UIC Class V Injection Wells. The proposed infiltration Field #1 system does not meet the minimum 10 feet setback to the building foundations (MSH Table RR and EEA Class V Well requirements). These foundations consist of 4 columns which support a 2,100+/- square feet three-story multi-family structure. The proposed infiltration system surrounding two of the four foundations could impose serious safety concerns with the building, two separate sets of State regulations (MSH and EEA) require a minimum of 10

feet setback between infiltration system/injection well and building foundations. Furthermore, the pretreatment device CB-2 does not meet the 10 feet setback to property line requirement.

A.) The proposed precast concrete boxes with open bottoms and set on top of a 12" crushed stone base function as an infiltration system. The open bottom would allow stormwater to flow beneath the building foundation and could affect the integrity of the building foundation. GCG recommends providing a minimum of 10 feet setback between the concrete boxes to the building foundations and property line.

B.) The eastern 120' long concrete box is proposed one foot from the face of building foundation wall. The applicant should verify the clearance between the building footing and the concrete box. There are multiple one (1') foot length drainpipe connections between concrete structures and building foundation. There is not sufficient clearance for proper compaction and access between structures. The subsurface infiltration systems should be designed to be reasonable for future repair and/or replacement.

C.) The infiltration system #2's top of stone elevation at 166.00 has less than a foot of cover over the system. There should be a 167 contour between the catch basin (CB-1) and the sewer oil & grease separator cover. CB-1 open grate was designed as an overflow device (see HydroCAD report node Pond 2P). But this catch basin grate is at the low point, with the closest and lowest discharge point to the garage entrance (elevation 167). It is illegal to discharge runoff to garage floor drain, which connects to the sewer line through oil and grease separator. Re-design required.

D.) The applicant should verify the ceiling clearance in front of the dumpster pad, the Architectural plan sheet A303 shown a clearance of 19'-6". The required dumpster pick height clearance is 24 feet minimum.

E.) Roof drain R-1 is undersized, the calculations (HydroCAD Reach 1R) used 8" diameter with 57% pipe slope, but the plan Reach Chart shows a roof drainpipe R1 8" pipe with 1.00% slope, furthermore, the DMH-1 outlet pipe R2 is 8" diameter at a 0% slope. Pipe R2 does not have the capacity to manage the roof runoff and will surcharge and overflow from DMH-1's cover due to the significant elevation head.

F.) The CB-1 outlet was labeled R6, which appeared to be R8. The DMH-3(SC) outlet pipe was labeled R5, which appeared to be R7. There is discrepancy between the DMH-3 outlet invert at 164.50 and the R7 beginning invert at 163.5. These two inverts should have identical elevation.

G.) GCG recommends increasing the roof drain and drainpipe R1, R2, R7, and R8 to 12" minimum diameter pipe. Where pipe flow is expected, pipe slope should be specified to provide sufficient self-cleaning velocity (2 feet per second minimum).

H.) This plan called for "**See landscaping plan for specific design layout, grading, & materials. Landscaping Plan will govern this area**" which includes the rear yard, fire lane, front patio, and pedestrian walkways. However, landscape sheet L-3 – Grading plan's Grading Note #1 stated that "**Grading on landscape plans is shown for reference – see civil drawings for final grades.**" GCG recommends providing one set of merged grading only and should be shown on the civil plan set and certified by the Civil Engineer. In addition, the landscape grading plan is creating a potential ice hazard

depression (spot elevation 175.95) in front of the sidewalk at the southeast driveway entrance. This spot grade is lower than the Massachusetts Avenue's gutter grade (176.4). The proposed spot grade also created a 22% ramp between the back of sidewalk to the civil spot grade at BC=177.2. The proposed vertical granite curb (VGC) along the permeable paver should be specified with the top of curb (TC) and bottom of curb (BC) spot grades to assure the surface runoff is controlled by the VGC. The proposed 167 contour in the rear yard, near the transformer pad, is causing ponding on top of CB-1. Additional grading is needed on the plan set. Spot grades should be provided along the west side of the transformer pad and at the main driveway to match the watershed drainage divide.

The Stormwater Management system has been revised. New comments are as follows:

- 8.1. Proposed Subsurface System #1 consists of two level stacked concrete chambers (4 rows of 6 units 3-feet in height each level), manufactured by Retain-It Stormwater Management System set on top of a 6" depth stone bed. The southeastern rows of 6 chambers are within the 10-foot property line setback and set on top of concrete slab as detention units without infiltration. However, the eastern 3 rows of chambers set on top of the 6" stone bed do not meet the two feet separation between the bottom of stone to estimated seasonal high groundwater (ESHGW) requirements. A minimum of two feet of soil should be provided for filtering treatments. Although plan detail (sheet C-7) claimed "No credit for infiltration for 6" Stone Base within 2 feet of groundwater", the stone bed underneath the 18 chamber units is the only surface (1,152 s.f.) connecting to the soil underneath for exfiltration and the sole outflow of the system. GCG recommends raising the bottom of stone bed to meet the minimum 2 feet separation to ESHGW requirements.
- 8.2. The chamber floor at the R-1 and R-3 inlets should be equipped with a 6" thick precast concrete splash pad as recommended by Retain-It manufacturer and adjust the reduced storage volume accordingly.
- 8.3. Subsurface System #2 consists of a single level of 18 2-foot height Retain-It chamber units, 10 of the units were proposed set on top of concrete slab without exfiltration to meet the 10 feet minimum setback to property line and foundation/footing requirements. Both subsurface systems 1 & 2 are shallow stormwater Underground Injection Control (UIC) Class V injection wells, and required to meet the MassDEP, Energy and Environment Affairs (EEA)'s setback to other subsurface discharge structures standard. Since the two systems exceed the maximum daily discharge rate of 2,000 gpd threshold, a 100-foot setback between the two basins applied. These two systems have 5-foot horizontal separation as proposed and should be 100 feet apart as designed. System #1 with stacked hydraulic head which is 6.5 feet could migrate to system #2 with the 5 feet horizontal separation and surcharge out of the WQ-1 catch basin grate. System #2's bottom of stone should be raised to provide the minimum two feet separation between the bottom of stone to ESHGW. See additional comments under the Stormwater Report below.
- 8.4. The proposed driveway apron (Sub-catchment 5S) created a new untreated runoff discharge flow toward Massachusetts Avenue, these peak flow and volume increases toward Mass. Ave. should be addressed.
- 8.5. The proposed area drains (AD1, AD2, and AD3) details should be provided. Pre-treatments prior to discharge to the subsurface system should be provided. Detail grading should be provided to ensure capturing all front yard (pavers) surface

runoff, GCG recommends relocating the area drains northward to the center of the plaza. The permeable paver design (runoff storage course and volume) and details should be provided. The Unit Pavers detail shown on Landscape Details, sheet L-4, do not provide any stormwater retention volume. The front yard permeable paver plaza appeared to serve as an outdoor seating area for restaurant use. Since there are outstanding insufficient parking issues. If the restaurant use is excluded, GCG recommends reducing the pavers surface with lawn or landscaped coverage to reduce the surface runoff.

- 8.6. Sewer Utility – the proposed Oil & Grease Separator shown at the northeastern building corner appeared to be proposed for the subsurface garage drain connection only. A properly sized Oil & Grease Separator would be required for restaurant use, if allowed, the separator location should be shown on the plan.
 - 8.7. The applicant should verify the ceiling clearance in front of the dumpster pad, the Architectural plan sheet A303 shows a clearance of 19'-6". The required dumpster front end pick-up height clearance is 24 feet minimum.
9. The drainage field design was based on the ESHGW elevation 161.0 found in TH-1, which was 0.5 feet lower than the water level recorded at the nearby monitoring well B3(MW) on the 5/28/2024 and much lower than the water level recorded in B1(MW) at 164.6, dated 4/23/2024. Additional soil test pits are required to determine the ESHGW. Resolved.
10. High point spot grades should be provided at the fire lane and garage access driveway along the Massachusetts Avenue right-of-way to match drainage watershed divide. Spot grades should incorporate with the sidewalk through driveway ADA/AAB cross slope requirements. There are discrepancies between landscape grading and civil grading on the plan set and should be addressed. The Area Drain (AD) details should be provided. AD1 and AD2 were proposed at the property boundary. Detail grading should be provided to ensure no surface runoff bypassing the area drains and created surface runoff flowing toward Massachusetts Avenue. GCG recommends relocating area drains toward the middle of the front plaza.
11. This plan showed grading within the rear yard along the MBTA ROW. The proposed grading in the rear yard area is part of the drainage overflow path to be designed to work with the overflow device CB-2. Proposed contour 167 should be shown on this engineer certified/stamped grading plan sheet. And should be reviewed by the engineer to assure the drainage overflow device (CB-2) rim grade works as intended. SMH-1's rim elevation as part of the utility design should be specified in the Civil plan set. (SMH-1's finish rim grade was not found on both plan sets). Landscape plan should be based on the Civil plan's grading design. The landscape grading along the rear yard does not allow the drainage overflow through CB-1. The proposed Subsurface Infiltration systems were designed with exfiltration outflow only. Excessive runoff beyond the design storm events appeared to overflow into the garage and discharge to the sewer line through the garage drain. The applicant should revise the contour 167 at the area between the dumpster and transformer pads, there should be an opening at 166.9 between the two BC=166.9 spot grades. The proposed contour 167 at the northern side of the transformer pad should be revised. Portion of the subsurface system #2 and the inlet water quality unit are below the three-story building bump out and future replacement clearance and complication should be considered.

12. Drainage structure chart's CB-1 invert out should be a single 12" HDPE only. (10" + 12" HDPE) were shown on the table, but the plan shown a single outlet pipe). [Resolved](#).
13. DMH-1's outlet inverts should be lower than the inlet invert to compensate the hydraulic loss through the concrete structure. Pipes with different sizes should match their pipe crown. [Resolved](#).
14. Reach Chart's, reach R-4's lower invert should be 163.75 to match the pipe slope and DMH-2 inlet invert. Reach R5 pipe size should be increased to 12" diameter to match R4 pipe diameter and HydroCAD model. Reaches R1, R2, and R3 pipe slopes are too steep, maximum pipe full flow velocity should be controlled below 10 feet per second, with maximum flow velocity (during partial flow at 80%+/- discharge capacity) to under 12 feet per second. [Verify R5 and R6 pipe labels \(duplicated\), see comment 8.F above. Resolved](#).
15. CB-2's open grate rim elevation as an emergency overflow outlet device should be lower than the CB-1 rim grade to avoid flooding in front of the garage. (Plan shown CB-1 and CB-2 rims have same elevation at 166.7). CB-1 rim grade is 0.3 feet lower than the subsurface garage entrance threshold at 167. During extreme storm events, the excessive surface runoff would pond on top of CB-1 to build up sufficient hydraulic head to push through the infiltration chambers system and overflow through CB-2. The applicant should calculate the peak water level on top of CB-1 and design the system accordingly. Stormwater overflows to the subsurface garage and discharge to the sewer line through garage floor drain is prohibited by State regulations. (See additional HydroCAD modeling comments below). [CB-2 eliminated, resolved](#).
16. The proposed 4-inch diameter garage sewer connection was intended to collect the subsurface garage's minor runoff from parked vehicles' snow/ice melt, dripping, and emergency spills, which would be treated by the oil & grease separator prior to discharge to the sewer line. The system is not intended to manage stormwater runoff. The site grading and drainage system should be designed to eliminate excessive runoff entering the garage floor drain. As shown, the surrounding top of curb elevations are higher than the garage entrance. Therefore, excessive runoff would be entering the garage and discharge through the floor drain to the sewer line, prior to overtopping the vertical granite curbs. GCG recommends providing an emergency overflow through the top of curb (by regrading) to release excessive water during extreme storm events. [See HydroCAD review comments. There appeared to be an opening at elevation 166.90 between the dumpster and transformer pads. See comment # 11 above, revise proposed contour 167 required.](#)
17. Provide estimate sewer flow calculations. Based on the 73 bedrooms and 5,000 square feet retail uses. The proposed 6-inch diameter sewer line should have the capacity to carry the expected peak flow. However, GCG recommends providing the existing 20" VC sewer main's upstream and downstream sewer structure inverts, so that the existing sewer invert at the proposed SMH-1 could be verified. [An estimate of 8,920 gpd sewage flow is proposed. Lexington Sewer Department approval of available capacity required.](#)
18. Water flow test should be performed to assure sufficient capacity and water pressure for fire protection. Fire service line should be shown on the plan. There is an existing hydrant within 200 feet of the proposed building on Massachusetts Avenue. [Fire Flow test report provided; Fire Department approval required.](#)

Construction Detail (C-7)

19. No weepholes should be allowed in concrete structures. (Precast Concrete Catch Basin detail drawing called for 4" diameter weepholes precast in base section.) All structures should be watertight. [Resolved](#).

Stormwater Report:

1. Based on the Existing Conditions Plan (C-1), there appeared to be an existing lawn depression in the rear yard of building #217 Massachusetts Avenue, where also retain and infiltrate portion of the predevelopment runoff. However, the local Wetland Protection Rules Chapter 130, Section 5. (6) has required the Pre-Development Conditions impervious area to be modeled as open space in good condition which should offset the existing infiltration function of the depression as indicated in the predevelopment HydroCAD report with zero outflow and volume during the 2-year storm event. Hence, GCG determined that the calculations met the intent of the predevelopment site runoff conditions. [Statement](#).
2. Post-Development calculations. The proposed stormwater management system consists of infiltration BMPs. The time span used in the HydroCAD calculations should be expanded to cover the full runoff volume, for a 24-hour storm, HydroCAD recommended a span of 0-30 hours. (5.00 -20.00 hrs. used). Furthermore, the time step (dt) should be reduced to avoid the oscillations errors/warning message during exfiltration calculations. GCG recommends using 0.01 to 0.02 hours dt, which would not trigger the warning message. [Resolved](#).
3. The post-development HydroCAD calculations utilized Woods, good condition coverage in small portions of the post-development watershed. GCG recommends changing the woods surface to lawns, based on the landscape plan, the planting areas are small and not dense enough to consider as woods. [Resolved](#).
4. There were approximately 3,173 square feet of permeable pavers used in the post-development calculations. However, Plan L-1's Materials Legend stated Unit Pavers (Porous within Riverfront), does that mean the remaining unit pavers are impervious? Plan L-1 also shows concrete pavement in front of the commercial spaces, and the spot grads in front of the building showing pitching toward the Massachusetts Avenue sidewalk and discharge onto the roadway which does not match with the Post-Development Drainage plan C-6 and the HydroCAD calculations. The landscape plan shows concrete pavement in front of the southeast building corner along Massachusetts Avenue. [Based on the landscape grading plan L-3. The front patio, walkway and fire lane drain toward Massachusetts Avenue and created an increased runoff rate and volume toward Massachusetts Avenue. \(\(HydroCAD report Link 2L\). This increase should be addressed. Sub-catchment 5S drains to Massachusetts Avenue. This is a new untreated discharge point to Massachusetts Avenue, which does not exist in the pre-development conditions. Although watershed area 5S is relatively small \(485 s.f.\), the discharge does not meet MSH standard #1 and does not meet the Town of Lexington Chapter 130, Wetland Protection, Section 5\(2\) – Increase in Runoff, the new discharge point will result in an increase in the total volume of the surface runoff for the 1-year return period storm to Massachusetts Avenue. There is no indication of the existing Massachusetts Avenue drainage system discharge point, However, the public street](#)

storm drain system would discharge to a protected resource area, applicant should verify the drainage system on Massachusetts Avenue.

5. MSH Vol. 2, Ch.2, Pg. 1187 stated permeable paving systems as part of the Porous Pavement and Vol. 3, Ch.1, Pg.15 stated that “Porous Pavement is considered to be an impervious surface for the purposes of calculating the required Water Quality Volume (WQV) and the Required Recharge Volume (RRV). When using porous pavement, the larger of the WQV or RRV must be used to size the storage media under the porous pavement.” Furthermore, porous pavement/paver should meet the setback requirements as listed in the MSH Vol. 2, Ch.2, Pg. 120. – including but not limited to Steep Slope less than 5%, 50 feet; Cellar Foundations, 20 feet; Slab Foundations, 10 feet; Property Lines, 10 feet. Porous Pavement storage volume should be designed according to the Storage Bed Design. (MSH, Vol.2, Ch.2, Pg. 121) and maintained accordingly per MSH. Porous Paver with stone storage course detail drawing should be provided. The porous paver was modeled with CN valid 70, which created increased runoff rate and volume toward Massachusetts Avenue, See comment 4 above. Sub-catchment 3S consists of 1,529 s.f. permeable pavers and 750 s.f. of Turfstone driveway. Both surfaces were assigned a CN value of 35 on the HydroCAD calculations, which is equivalent to the previous woods’ coverage surface (between good (CN 30) to fair (CN 36) woods conditions. The assigned CN 35 value is acceptable with proper permeable pavers or Turfstone surface design. GCG recommends resolving the discrepancy between the Civil sheet C-2, (which calls for pavers) and the Landscape Plan sheet R-1, (which calls for concrete pavement) at the exterior bike racks and sitting area in front of the southerly building corner. Only permeable pavers should be used to match the HydroCAD calculations. The permeable pavers and Turfstone cross-section details should be equipped with a stone reservoir layer should be sized to resemble the CN 35 value. The Landscape Details plan sheet L-4 shows a Unit Pavers detail with pavers set on top of a 1 ½” sharp sand setting bed over a 12” dense graded aggregate is part of the drainage system and should be sized and detailed by the engineer. The proposed “3/8” max joints (smaller is acceptable)” label shown on the Unit Pavers detail should have the minimum pavers joint opening specified; Sub-catchment 4S utilized 4,884 s.f. of Woods surface coverage in ‘Good’ conditions. Since there are existing Right of Way and Sewer easements over the rear yard, Woods coverage would be limited. GCG recommends using Open Space – Good conditions (CN=39) to compensate for the Fibar path and shrubs in the rear yard.
6. HydroCAD Pond 1P infiltration Fields appeared to be undersized. The top of stone storage is at elevation 166.21. The 100-year storm event peak elevation was calculated at 166.74, which is above the storage volume. (Hydrograph output above the assigned storage volume is invalid per HydroCAD warning message.) GCG recommends modeling the system with the ponding volume on top of CB-1, (volume enclosed by the garage entrance threshold and top of curb elevations) to evaluate the actual peak elevation with the emergency overflow device (CB-2), CB-2 overflow path should be equipped with erosion protection and/or utilizing level spreader and avoid concentrated discharge point. HydroCAD Pond 1P, outlet devise #2 utilized two (2) – 2’ x 2’ Horizontal office/grate, which included CB-1 and CB-2 top grates. However, CB-1’s open grate does not have an outflow path and should not be qualified as an outlet device. CB-2’s emergency overflow path should depend on the proposed grading around the open grate and should be modeled accordingly. As modeled, the open grate should be set at the top of the slope and allow flow from all four sides. Subcatchment 2S shows 396 s.f. of grass area, GCG scaled the two narrow landscape island and measured approximately 80+/- s.f.; Subcatchment 3S should include the 440+/- s.f. concrete pavement in front of the

southeast building corner; Subcatchment 5S appeared larger than 51 s.f.; Reach 1R pipe slope at 0.5750 ft/ft (57% slope) should be verified, (plan shown R2 with 0% slope); Reaches 4R and 5R do not match with the pipe slopes shown on the plan; Pond 1P, single outlet 12" horizontal Orifice/grate should be double, the applicant should verify the downstream 12" pipe laid level has the capacity to handle the orifice flow; Pond 2P – Field #2 consists of 22 Cultec C-100 chambers embedded within a combined 91.75'L x 9'W x 2'H stone bed. The plan shows 56.75'L x 9'W x 2'H trench with 14 Cultec HD-100 chambers. The system is undersized as shown on the plan. The top of the storage volume elevation is at 166.00 and the 25-year storm peak elevation is at 166.71, which is higher than the system storage. The proposed 2'x2' horizontal orifice/grate overflow outlet at elevation 166.70 (CB-1) is above the storage volume, and the CB-1 grate is trapped in the low spot with no outlet. These calculations need to be reviewed and corrected. The subsurface infiltration system has been split into two fields, the minimum separation between the fields is not met, (a 100 feet separation is required) See comment item 8.3 above. The applicant should verify the HydroCAD modeling for the infiltration system Pond 1P and Pond 2P. Pond 1P consists of the stacked Infiltration System (Field #1) to retain and infiltrate the roof runoff. However, the Pond 1P's calculated infiltration/discarded rate appeared to be excessive. Pond 1P consists of 1,152 s.f. of stone bed surface area with the 0.003347 feet per minute exfiltration rate, which calculated the maximum exfiltration/discarded rate over the surface area to be 0.06 cubic feet per second (cfs). But this HydroCAD report showed 0.06 cfs, 0.08 cfs, 0.13 cfs, and 0.14 cfs exfiltration/discarded rates for the 2-yr, 10-yr, 25-yr, and 100-yr storm events, respectively. Since the exfiltration is fixed, the infiltration rate should not exceed 0.06 cfs. In comparison, Pond 2P (field 2) has a constant infiltration rate of 0.03 cfs, which reflects the exfiltration through the stone bed surface area below the 8 chamber units only. Furthermore, based on the "retain-it module storage volumes & weight by height" sheet provided by retain-it Stormwater Management Systems. The storage volume used in the HydroCAD calculations appeared to be conservative. GCG recommends revising the HydroCAD modeling for the infiltration systems (utilizing the bottom of the stone bed surface area for exfiltration only) and resolving the two systems separation/setback issue.

7. Both infiltration BMPs (Pond 1P, Infiltration Field (field 1) and Pond 2P, stone base under Fire Lane) do not meet the required 10 feet setback to building foundation and property line (stone under Fire Lane system), calculations should be revised accordingly. See comments 4 and 5 above.
8. Where bottom of infiltration system does not meet the minimum 4 feet separation to ESHGW should provide water Mounding Analysis per MSH, Vol. 3, Ch.1, Pg. 28. Water mounding calculation should be provided. The proposed precast concrete boxes storage volume consists of 3,808 c.f., (the chamber will be filled to elevation 165.96, 0.04 feet below the top of the storage volume during the 100-year storm event). Majority of this volume is designed to discharge through the two 1" orifices connecting to Field #2 which is substantially undersized for infiltration. The applicant should provide a draw down calculations to prove the system could empty out within 72 hours. Mounding calculations should be revised with systems separation setback resolution.
9. There are discrepancies between the ESHGW elevation found on soil test pit TH-1 and the on-site monitoring wells readings. TH-1 also identified medium and fine sand material, but the soil boring logs by GEI called out silty sand in their logs. The applicant should request through GEI and submit the Garin Size Test reports to support the site soil's exfiltration rate used in the calculations. Additional soil test pits should be

performed to determine the ESHGW across the existing steep grade. Test pits should be witnessed by the Town or its assignee. GCG concurs with the soil test results. Resolved.

10. MSH Standard #8 - Construction period Operation and Maintenance (O&M) plan, (shown in plan sheet C-4) should be duplicated in a separate document and suitable for the construction period responsible party's signature. This project is below the 1-acre land disturbance threshold and does not require an NPDES CGP. Statement.
11. MSH Standard #9 – Long-term O&M plan should call out the catch basin grate and sump should be inspected at least 4 times per year. (3 times per year specified). The inlet grate should also be cleaned four times per year. Porous paver maintenance should be added to the O&M plan. O&M plan should be modified in corporate with the future drainage design changes.
12. Total Phosphorous (TP) calculations should be provided. Downstream receiving water Mill Brook is classified as impaired water with TDML requirements. TP removal calculations are not provided. Resolved.
13. During the 19-18-2024 site visit, GCG observed a low point/depression within the paved bikeway behind the development site. Any drainage overflow discharges onto the bike path should address any potential water ponding and icy hazardous conditions during cold weather months. The proposed Field #2 overflow grate (CB-1) has no outlet and is invalid. Infiltration Systems minimum separation should be addressed, system calculations should be revised per comment #6 above.

Conservation Commission Notice of Intent Comments:

The site is in the outer 100' to 220' riparian zone of the Riverfront Area of Mill Brook (impaired water). The report did not indicate any Bordering Vegetated Wetland (BVW) associated with the Mill Brook bank; the wetland delineation needs Conservation Commission approval. The Riverfront Area is partially degraded, the total Riverfront Area is 13,212 s.f. The existing degraded or impervious area is 6,316 s.f. The project proposes an additional 6,896 s.f. of impervious area within the 200 feet Riverfront Area. The future (offsite -MBTA property) proposed an additional 800 s.f. of disturbance. The total proposed work within the 200 feet Riverfront Area is 7,776 s.f. The applicant has proposed 16,000 s.f. of mitigation, which is the is invasive species management for Japanese Knotweed (*Polygonum cuspidatum*) and Norway Maple (*Acer platanoides*) on the parcel Map 13 Lot 383, (2.36 Acres), owned by the Town of Arlington, located at the northern side of the Minuteman Commuter Bikeway, with mitigation ratio of or greater than 2:1. No alternative analysis has been provided which is required for Conservation Commission review and approval. Under 310 CMR 10.58 (4. The NOI narrative stated that Norse Environmental Services, Inc., (NES), confirmed and agreed with the Band flags flagged by LEC. The applicant should clarify if any BVW resource area was identified with the river Band and provide the associated MassDEP BVW Field Data Form(s) to support the finding. If no BVW is identified, which should be stated in the report.

The applicant has provided four alternative analyses for the proposed development within the 200 Riverfront Area which the commission should review and discuss. The alternatives are as follows:

1. First alternative - Reduce the scope of the project and relocate the mixed-use building outside the 200 ft. Riverfront Area. This alternative will reduce the building footprint by 58% (7,396 s.f.) and is considered cost prohibitive.
2. Second alternative – to Comply with 310CMR 10.58(5)(e) – The area of proposed work shall not exceed the amount of degraded area. The applicant stated that the 4 lots combined total 30,866 s.f., with 13,212 s.f. in the outer riparian area and existing 6,316 s.f. of degraded area. The redevelopment of these properties requires a new building footprint, drive aisles, onsite parking, and drainage improvements. The 5-story mixed-use building would be eliminated to comply with 310 CMR 10.58(5)(e). This alternative is cost-prohibited.
3. The third alternative is to reduce the size and scope of the project and eliminate the retail/restaurant space. And reduce the building story to three-story with the similar building footprints. However, this alternative still requires work within the 200 Riverfront Area and offsite mitigation measures to comply with the Riverfront Regulations. The Town strongly encouraged the (5) story mixed-use multifamily retail/restaurant rath than the 3-story residential option.
4. The fourth alternative is to eliminate the mixed-use residential building/restaurant space and construct Commercial Retail Shopping (CRS). This alternative will also encroach with the 200 ft. Riverfront area and require offsite mitigation. This alternative does not address the intent of the Village Overlay district as part of the MBTA housing directive to increase residential housing supply.
5. The last and preferred alternative is the site plan as presented with the mixed-use building with off site improvements.

The existing site consists of 13,212 s.f. Riverfront Area (outer riparian, 100' to 200' from the riverbank) with existing 6,316 s.f. degraded area (impervious surface consists of hot mix asphalt pavement and accessory structures/garages and sheds. This development proposed approximately 8,500+/- s.f. of combined building, pavement, concrete pad, and pavers area, (impervious surface) and approximately 530+/- s.f. of play structure “safety surface” and 750+/- s.f. of Fibar – engineered wood chip accessible pathway (semi-pervious) within the Riverfront Area. A net increase of approximately 2,200+/- s.f. of new impervious area in the riverfront area. The new 530+/- s.f. of safety surface and the 750+/- s.f. Fibar pathway are semi-impervious. The project has proposed to improve an off-site Riverfront Area (Map 13 Lot 383, owned by the Town of Arington) by removing invasive vegetation and placing tree and shrub plantings on approximate 10,450 s.f. area. (Approximate area scaled from plan set).

310 CMR 10.58 (5)(g) - Mitigation and restoration of degraded riverfront area either on-site at ratio in square feet of at least 1:1 or in the riverfront area within the same general area of the river basin, alternation may be allowed at a ratio in square feet of at least 2:1 is allowed under 10.58 (5)(g).

The restoration required under 10.58 (5)(f) were stated as:

1. removal of all debris but retaining any trees or other mature vegetation.
2. grading to a topography which reduces runoff and increases infiltration.
3. coverage by topsoil at a depth consistent with natural conditions at the site; and
4. seeding and planting with an erosion control seed mixture, followed by plantings of herbaceous and woody species appropriate to the site.

This project has proposed 8,500+/- s.f. impervious areas (6,316 s.f. was existing degraded impervious surface) and 530+/- s.f. of safety surface and the 750+/- s.f. Fibar pathway within the outer riparian riverfront area on-site (the safety surface and Fibar wood chip path are considered pervious surface, but not natural surface) and the plans propose restoring approximately 10,450 s.f. of invasive species vegetated off-site area within the same general area to native plantings coverage. Typically, increased impervious surfaces are restored by removal of impervious area on-site or in the same general area. The Commission should determine the proposed restoration meets the intent of an equivalent level of environmental protection.

310 CMR 10.58(4)(c)1. - The alternative's practicable and feasible should be considered by the Commission based on the four factors: a. Costs; b. Existing Technology; c. The Proposed Use; and d. Logistics as described in 10.58(4)(c)1.

The development site is partially in the outer 200 feet Riverfront Area. The existing site surface runoff discharges to the Riverfront Area without any drainage treatment. Hence, it is subject to the Stormwater Management Handbook Standards and Regulations. A Checklist for Stormwater Report should be submitted. A Checklist for Stormwater Report stamped and signed by a P.E. should be included in the package.

Local Wetland Protection Code Chapter 130:

Chapter 130 - Section 5-(6): The pre-development conditions drainage calculations utilized the 'Open Space in Good Conditions' for the existing structures and impervious groundcover to be demolished, removed, or otherwise taken out of service requirements. The pre-development calculations met the requirements, see Stormwater Report comment #1 above. **Statement.**

Chapter 130 – Section 5-(2): The post-development drainage calculations indicated reductions in peak discharge rates during the 2-year, 10-year, and 100-year return period storm events and decreased total surface runoff volume for the 1-year return period storm at all design points which complies with the intent of Section 5-(2). There are indications of increased peak runoff and volume toward Massachusetts Avenue with the post-development conditions. (1 year storm event stormwater runoff volume not included in the pre-development and post-development calculations.) However, there are some major drainage calculations discrepancies not matching with the plan set and the drainage calculations will need to be corrected and modified as plans are changed. GCG is unable to determine the drainage compliance with the Massachusetts Stormwater Handbook nor Section 5-(2) of Chapter 130. **Based on the latest stormwater report, there is a net increase of surface runoff flows and volume southward to the Massachusetts Avenue catch basin system from post-development sub-catchment 5S. This sub-catchment surface runoff drains northward in the pre-development conditions. Although the peak flow rates from sub-catchment 5S were small, (0.02 cfs during 2-year storm event to 0.09 cfs and during 100-year storm event), but these are untreated new discharge to Massachusetts Avenue. GCG recommends collecting the sub-catchment 5S surface runoff to the subsurface infiltration system with pre-treatment. Stormwater should be managed according to standards established by the Department in its Stormwater Policy. (10.58(4)(d)1.b.)**

Please call with any questions.

Respectfully Submitted,
GCG ASSOCIATES, INC.

Michael J. Carter

Michael J. Carter, P.E.
President



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Abby McCabe, Planning Director
Meghan McNamara, Assistant
Director
Carolyn Morrison, Planning
Coordinator



To: Lexington Planning Board

From: Meghan McNamara, Assistant Planning Director

Re: Site Plan Review and Special Permit for 217-241 Massachusetts Ave.; Village High-Rise Overlay District (Memo #3)

Date: January 24, 2025

The Applicant submitted the following material for the public hearing on January 30, 2025: exterior light schedule/spec sheets, ANG worksheet, ANG spot grade plan, response to Bicycle Advisory Committee (1/9/25), const. mgmt. plan (rev. 1/6/25), planset (rev. 1/9/25), response to peer review memo (1/9/25), response to staff memo (1/9/25), riverfront mitigation plan (1/2/25) and stormwater report (rev. 1/9/25).

A memo from peer review consultant, Michael Carter, PE, GCG Associates, dated 1/23/25 is provided as a separate letter.

In addition to public comment letters, comments were also provided from the Design Advisory Committee, who met on November 19, 2024.

Applicant filed a Notice of Intent with the Conservation Commission and the first public hearing scheduled on November 18, 2024. It is recommended that the Planning Board not close the public hearing until the Conservation Commission hearing is closed. The project applicant returns to the Conservation Commission on February 3, 2025

Project Summary:

North Shore Residential Development, Inc. proposes to construct a new 44-unit, 5-story mixed-use / residential condominium building with residential parking for 44 cars (38 interior and 6 surface/interior parking spaces) as well as an additional 8 exterior commercial spaces. The proposed five-story building is planned to have a 15,090 square-foot footprint at ground level and a total floor area of 75,461 SF. The proposed building will consist of one below-grade parking level, a mixed-use ground floor, and four upper levels dedicated to residential space. Uncovered retail parking and a playground are also planned to be included as part of the development. The (revised) proposed connection to the Minuteman Bikeway is via the existing bluestone walkway from the bikeway, and a pathway further connecting to Mass Ave. Applicant is required to provide 6 Inclusionary Dwelling Units (IDU) limited to residents earning 80% of the Area Median Income, and is proposing 6 units.

Planning Staff Comments:

- Appreciate the Applicant providing more short-term bike parking beyond what is required. We hope that additional bike parking can be added along the MMBW in the future with approval from the MBTA, to promote additional modes of transportation and support future commercial tenants.

- Staff confirm that the proposed building complies with Zoning setbacks.
- Applicant will explore the possibility of a day-time on-street loading zone. This will need to be approved by the Select Board.
- Applicant is pursuing an all-electric passive house certified building and will consider participating in Lexington's composting program.
- Provide a landscape plan that labels all proposed plant species. The Planting Plan refers to the civil site plan for rear property, however the civil plan does not label any of the proposed trees and/or shrubs. Please combine onto one landscape plan.
- Staff are not supportive of the 16-foot high pole mounted light fixture in close proximity to Mass Ave. Please revise with a lower mounting height. Photometrics plan still shows light trespass onto the Minuteman Bikeway and Massachusetts Avenue. Minimal light trespass is also shown onto the abutting properties. Applicant shall revise the photometrics plan to eliminate all light trespass. Applicant can request a Special Permit to allow light trespass onto the public right of way however this would require readvertisement. Applicant acknowledges that any overnight lighting will be no greater than 0.5 foot-candles on average and will be on a timer. All proposed lighting has a CRI greater than 50 as required. Outdoor lighting shall use 3,000K or warmer fixtures and 2,700K is encouraged.
- Applicant will be required to comply with Ch 135 Section 5.2 Signs, as amended through 2024 Annual Town Meeting. Signage is proposed for the commercial tenants and residential address numbering. Future commercial tenants should be made aware of signage regulations along the Minuteman Bikeway, in addition to projecting signs, awning signs, wall signs and illumination.
- There is a large tree in front of 241 Mass Ave. that is not shown on plans. Update plans to show this tree and mark for removal. Also include species and DBH inches.
- As noted during previous Planning Board meetings, there is great concern from the neighborhood for the 48" tree on the property of 251 Mass Ave. Request that a certified arborist be on site when any site cutting or disturbance is taking place within close proximity of it's roots.
- The portion of the site that was previously proposed as a fire truck access lane is no longer needed per the Fire Dept, so the Applicant should revise plans to incorporate as little site disturbance in this area as possible.

Staff Recommendation:

At the end of the hearing, staff recommends the hearing be continued to a Planning Board meeting in February or March. The applicant should revise their plans to comply with the Conservation Commission's Riverfront Requirements and the town's Stormwater Management Regulations prior to returning to the Planning Board.

Findings:

1. Pursuant to 135-7.5.5.6: the minimum required front yard in feet is the lesser of that required in the underlying zoning district or 15 feet, except that where 50% or more of the façade facing the public way is occupied by nonresidential principal uses, no front yard is required. Staff conclude that the majority of the street-facing level of the proposed development is proposed as commercial space. Architectural plans show 60-feet 4-inches of nonresidential facade facing Mass Ave, and 40-feet 8-inches of residential façade, meeting the greater than 50% requirement.
2. Per 135-7.5.5.10(a) – In the VO District where the underlying zoning district is a Commercial District, the maximum height is 60 feet when at least 30% of the gross floor area of the street

floor level or buildings' footprint, whichever is greater, of the entire development is occupied by commercial principal uses, not including associated parking, shown in Table 1 §3.4 Permitted Uses and Development Standards as permitted in the underlying district in the following categories: Commercial Office Uses; Personal, Business or General Services Uses; Sales or Rental of Goods and Equipment; Eating and Drinking; and Commercial, Recreational, Amusements, Entertainment. The Applicant is proposing 4,586 SF of commercial space on the first floor, exceeding the required 4,527 SF to meet the 30% requirement. [4586 SF / 15090 SF (ground floor footprint) = 30.39%]. The current proposal meets the criteria to receive the height bonus.

Proposed Unit Count:

(X/X) = (Market Rate Total/IDU Total):

Unit-Type	Total Number	Bedroom Count	Average Unit Size SF)	Percentage of Total
1-Bedroom	17 (15/2)	17	943	39%
2-Bedroom	23 (20/3)	46	1229	52%
3-Bedroom	4 (3/1)	12	1755	9%

Special Permit Requested:

- Pursuant to §135-7.5.6.3 to allow fewer parking spaces than required for the commercial use(s). Because this is a walkable area with nearby on-street parking, MBTA bus service, and located on the Minuteman Bikeway, staff supports the reduction of required number of commercial spaces. Requirements for commercial vehicle parking based on use are provided in the table below.

Relief Required:

- 135-5.1.13.1(e) all vehicles may exit from and enter onto a public street by being driven in a forward direction and no vehicle shall be required to enter or leave by backing [up]. The current trash and recycling removal plan requires a truck to exit the site in a reverse direction, unless the Applicant can provide evidence proving otherwise.
- 135-§5.1.13(1a)(*) to allow parking spaces that abut a wall to have less than a 12-foot width. Of the 38 garage parking spaces, two (#15 & #16) do not meet the 12-foot width requirement, and are instead 11'-8" and 10'-0". Staff believe this request is justified because drivers pulling into these two spaces can pull all the way through, eliminating the need to drive in reverse to exit a space.
- 135-§5.1.11 (3): to allow parking within the 10-foot setback from the street line. Applicant proposes one van accessible parking space in close proximity to Mass Ave. The space where the vehicle will be parked is 12 feet from the street line, while the required access aisle at its closest is 4-feet 2-inches. Applicant requests a waiver if the Board considers the access aisle to be a parking space.
- 135-§5.1.12(1) and (3a) and (3b): to allow proposed landscaping to serve as screening because this project is across the street and across the Minuteman Bikeway from Residential Districts. Design includes a 4-foot landscape buffer, however applicant notes any plantings taller than 30" would cause visibility issues for cars entering and exiting.
- 135-§5.1.13 (1c) to allow structural column to be within 3 feet of the drive aisle. At it's closest, the distance between the drive aisle and columns is ± 2'2"

- 176-12.9.5(8) use of stormwater harvesting systems, such as cisterns and ponds, for plant irrigation. Applicant notes that there is insufficient land area to support a pond, and has incorporated LID such as pervious pavers, restoration area plantings, and subsurface infiltration.
- 181-73B(2)(e) water velocity shall not be more than 5 feet per second (fps) on paved surfaces. This is a waiver request of the DPW Stormwater Management Regulations performance standards. This is noted in the peer review memo, general comment #6. Plans may be revised to eliminate this waiver request.
- 176-12.4.2(7) each bicycle parking space shall be at least six feet by two feet. In an effort to accommodate oversized bike parking spaces requested by the Bicycle Advisory Committee, the Applicant proposes 28 bike spaces utilizing the dero-decker system, which do not meet the 6x2 requirement.
- 176-12.4.2(9) parking shall not require lifting bicycle off the floor or carrying bicycles up or down stairs, whether indoors or outdoors. See note above
- 176-12.4.2(4) Bicycle racks shall be easily accessible for both long-term and short-term use. In an effort to provide a a combination of at-grade bike spaces and oversized bike spaces, there are structural columns located in the bike storage room that may interfere with easy access to the stacked bicycles and some of the at-grade spaces.
- Chapter 120 Tree Bylaw & Street Trees Under existing conditions, the project site does not contain any trees. There are however 7 trees between the rear property line and the bikeway, and two trees along the frontage of Mass Ave within the right of way. The limit of work line in the rear of the property does not extend beyond the property line. Adequate signage shall be installed to inform construction crew that the bikeway trees are not to be removed or pruned. Along Mass Ave, a 2-inch tree is proposed for removal.
- A 16-inch street tree (between #229 & 233 Mass. Ave.) is proposed to be saved, with a comment that if construction activites cause harm to the tree it shall be removed. **Please include a note on all civil plans that this tree is proposed to be saved and show adequate protection.**
- Applicant proposes 4 red maples along the frontage of Mass Ave and 6 unknown native tree species along the rear property line. Staff recommend waiving jurisdiction of the Tree Bylaw to the Planning Board under Site Plan Review.



Vehicle and Bicycle Parking:

Parking Type	Amount Required		Provided	Notes
	1.5 per dwelling unit	66		
Long-Term Bicycles	1.5 per dwelling unit	66	66	Waiver req. for lifting and space dimensions
Short-Term Bicycles	0.1 per dwelling unit	5	9	In Compliance
Residential Vehicle Spaces	1 per dwelling unit	44	38 inside & 6 outside	Waiver req. for 3-foot wall offset

Commercial Vehicle Spaces ³	Eating Establishment: 1 per 5 seats or 1 per 200 SF of Net Floor Area ¹ , whichever is greater	18.34	8	See Special Permit request
	Retail: 1 per 600 SF of Net Floor Area ¹	6.12	8	
EV Parking ²	Minimum 4%	2	8	In Compliance

¹- See Ch. 135 (Zoning Bylaw) Section 10.0 for definition of Net Floor Area. In brief, net floor area is the total square feet of occupiable or habitable area in a building determined by using 80% GFA. Applicant proposes 4,568 SF of commercial GFA, or 3,669.2 SF of net floor area.

²- Per 135-5.1.13(11): the parking lot shall be constructed with appropriate conduits and space for transformers and switchgear to allow for future installation of EV charging stations for a minimum of 50% of total off-street parking spaces

³- Per 135-7.5.6.2: the parking factor for all other uses shall be the same as in § 5.1.4 (Table of Parking Requirements) for the CB District

Maximum Height:

The proposed height is 59’11” The Building Commissioner is still reviewing the Average Natural Grade and Elevations of the proposed building to confirm building height complies. This will also be reviewed and confirmed prior to issuance of a building permit. Applicant to confirm roof top elevation used is to the parapet, architectural plans look like the height to mechanicals is 59.98 feet, it’s not clear to where this is measured.

Building and Zoning:

- Please add the heights and setbacks for all retaining walls, walls need to be setback the distance equal to the height
- Show details and height of rooftop screening , structures on the roof are limited in area
- Access from van accessible space near mass ave should not require passing across driveway for entry to building, review adding access to sidewalk
- Confirm accessibility throughout the site, such as paths, the slopes, gates, ground materials, playground equipment, surfaces etc..
- Mulch and/or stone dust is not an approved durable surface per ADA/AAB for persons with a disability, please review and review with Accessibility consultants
- Variances are available from the AAB, or official interpretations, any surface other than paved or concrete will require their review
- Is the tree labeled to be removed on mass ave a town tree, check with tree warden for permission
- For generator, add setbacks and confirm the generator will not result in a noise bylaw violation, is there a better place that will not effect neighbors?
- Buildings foundation is on sewer easement line, confirm no issues with access to easement with regards to structural concerns of building foundation for future easement access, I don’t think they’ve responded to engineering's question
- If access to bike path is not ADA compliant, I suggest a variance from state or eliminating the access/gate
- Confirm Permeable pavers are ADA compliant

- ANG points are submitted, just need highest point of roof/structure, to confirm height, can you create and provide the summary of the elevations on the zoning elevations/height form?

Fire:

- Applicant was notified on 1/21/25 that they are not required to provide the 50x15-ft fire access lane adjacent to 251 Mass Ave because a 250-foot fire hose can connect around the perimeter of the building from a truck parked on Mass Ave. A fire truck can enter and exit the site from the eastern curb cut if necessary. Applicant will be presenting proposed concepts for this area to activate this pedestrian way at the next meeting.
- Applicant confirms that the drainage system underneath the driveway and parking area are rated for H-20+ loading, and do not require any cover to support traffic loads but has provided 1 foot of cover of the system.
- Exterior access to a sprinkler room must be provided.

Engineering:

Engineering Staff do not have any concerns with the latest planset. The proposed shrub plantings within the sewer easement are acceptable. Staff have approved parking of construction vehicles in the sewer easement only during site construction period.

Important Dates/Timelines	
Public Meeting	September 25, 2024, continued to November 20, 2024 and further continued to January 30, 2025
Filed with Town Clerk	August 20, 2024
Decision Deadline (150 days)	February 12, 2025

Draft Findings and Conditions of Approval for consideration at future meeting:

1. Applicant to construct bumpouts (curb extensions) on both sides of Mass Ave at the crosswalk in front of 241 and 250 Mass Ave. Applicant will be required to receive a street opening permit through the Engineering Department prior to the start of work. Staff request that the bumpouts be installed and crosswalk repainted if deemed necessary by staff prior to issuance of the first Certificate of Occupancy. Crosswalk bumpouts at this location are a top priority on the Town Wide Bicycle and Pedestrian Plan. Additional coordination with Planning Staff will be necessary prior to construction.
2. The applicant shall coordinate a meeting with engineering, public safety, and assessing staff to determine address assignments. Town Staff will create new addresses and notify applicant of when final address assignments are created in the systems.
3. There is no overnight street parking allowed in Lexington on public ways. Prospective owners should be made aware of this prohibition. This should be outlined in the condo/HOA documents.
4. While developing the site, must have a Pest Management & Dust Control Plan in place.
5. We request the developer commit to include a welcome packet with information about no overnight on-street parking, the MBTA, The Rev shuttle, Lexpress and the Minuteman Bikeway

with each new purchase and that this information be kept up to date and available in the common space. We also ask that the developer commit to installing an electronic transit screen in a common area for live timing on MBTA bus services, and other transportation services if available in the future.

6. Applicants shall meet all requirements of the Noise Control Bylaw (Chapter 80) and as amended during the 2024 Annual Town Meeting.
7. As a condition of approval, and to be included in DEED/HOA/lease documents, that all dwelling units, including IDU, are prohibited from being used as short-term rentals (6.10.3(2)).
8. Before a building permit is issued the Applicant will need to submit an application for an ANR combining the lots and then record the new plan prior to the issuance of a building permit, this can be a condition of approval.
9. In order to properly manage, maintain, rehabilitate and or replace the existing system requires accessibility of heavy equipment suitable for the excavation and replacement of large capacity pipe with structural impacts to proximate facilities. Stockpiling of materials along the pipeline during construction also will not be permissible.
10. Applicant to provide the Town of Lexington with a public access easement over the property to provide access to the bikeway.
11. Work to replace the water line under the Minuteman Bikeway (MMBW) begins next spring. The MMBW will be detoured on to Mass. Ave. which may impact this project's ability to use the ROW. The Applicant will need to coordinate with MWRA and the Lexington Bicycle Advisory Committee.
12. Applicant will be responsible for any police details needed during construction and for re-routing pedestrians.
13. Applicant must apply to the Tree Warden for a public shade tree hearing if proposing to remove a public shade tree.

AGENDA ITEM SUMMARY

LEXINGTON PLANNING BOARD

AGENDA ITEM TITLE:

7 Hartwell Avenue - Public Hearing

PRESENTER:

Applicant: Dinosaur Capital Partners
LLC

ITEM NUMBER:

SUMMARY:

The Planning Board will open the public hearing on the application of Dinosaur Capital Partners LLC, for approval of a major site plan review under §135-7.5 [Village & Multi-Family Overlay Districts] and 9.5 [Site Plan Review] of the Zoning Bylaw and Article VI of §181-71 Stormwater Management Regulations. Proposal also requests a special permit, pursuant to Section 5.1.14 and 5.1 [Off-Street Parking and Loading]. Application is to construct a 130-unit multifamily property in a five-story building with a 300 SF café, 203 indoor bicycle parking spaces and 14 outdoor visitor bicycle parking spaces, 90 underground and 40 surface automobile parking spaces, communal tenant amenities, and improved site amenities.

The property is located at 7 Hartwell Avenue, Lexington, MA also known as Map 84, Lot 81 in the CM (Manufacturing) and VHO (Village High- Rise Overlay) zoning districts.

Application materials may be viewed at (Click files tab): <https://lexingtonma.portal.opengov.com/records/99194>

A staff memo and memo from the Planning Board's hired peer review consultant are attached.

The Applicant's team will present, staff will summarize comments, board members may ask questions and discuss before opening the hearing for public comments. The Chair will then open the hearing for public comments. The applicant, board, and staff may then discuss further before continuing the hearing to a future meeting date.

SUGGESTED MOTION:

FOLLOW-UP:

DATE AND APPROXIMATE TIME ON AGENDA:

1/30/2025

ATTACHMENTS:

Description	Type
☐ Peer Review Memo - 7 Hartwell Ave	Cover Memo
☐ Staff Memo	Cover Memo

HANCOCK ASSOCIATES

Surveyors | Engineers | Scientists

January 23, 2025

Lexington Planning Board
c/o Abbigail McCabe, Planning Director
1625 Massachusetts Avenue
Lexington, MA 02420

Subject: Planning Board Project No. PLAN-24-26
7 Hartwell Avenue, Lexington, MA
Peer Review Letter #1

Dear Chair Schanbacher and Board Members,

Hancock Survey Associates, Inc. (Hancock) has contracted with the Lexington Planning Board (Board) to review a Site Plan Review and Special Permit application filed by Dinosaur Capital Partners LLC for a proposed development at 7 Hartwell Avenue.

Hancock's review comments are intended to assist the Lexington Planning Board in understanding the proposed project, to identify the technical issues relating to the stormwater design, and to make recommendations to the Town of Lexington (the Town) for possible technical improvements to the proposed project.

Hancock received the following documents and plans:

1. A Site Plan Review plan set entitled, "7 Hartwell; Lexington, MA; Dinosaur Capital Partners," compiled by Icon Architecture, dated 1/6/2025 which includes
 - o An architectural plan set, prepared by Icon Architecture;
 - o A civil plan set, prepared by Nitsch Engineering; and
 - o A landscape plan set, prepared by Copley Wolff Design Group;
2. A stormwater report entitled "Stormwater Report; 7 Hartwell Avenue Housing Development," prepared for Dinosaur Capital Partners, LLC, prepared by Nitsch Engineering, dated December 5th, 2024; and
3. A letter entitled "7 Hartwell Avenue; Project Narrative," dated 12/10/2024 (per filename);
4. A letter entitled "7 Hartwell Avenue; Zoning Narrative," dated 12/10/2024 (per filename);
5. A memorandum entitled "7 Hartwell Avenue Sewer and Water Calculations," prepared for Lexington Engineering Department, prepared by Nitsch Engineering, dated December 9th, 2024;

6. A Lexington form entitled “Form W; Application for Waivers,” applicant 7 Hartwell LLC, dated 12/10/2024 (per filename);
7. A report entitled “Construction Management Plan; 7 Hartwell Ave Housing Development,” prepared for Dinosaur Capital Partners, LLC, prepared by Nitsch Engineering, dated December 3rd, 2024;
8. A letter entitled “7 Hartwell, Notice of Intent,” dated 12/10/2024 (per filename);
9. A report entitled “DRAFT Stormwater Pollution Prevention Plan (SWPPP),” prepared for Dinosaur Capital Partners, LLC, prepared by Nitsch Engineering, dated 11/25/2024; and
10. A memorandum entitled “Transportation Fact Sheet; 7 Hartwell Avenue, Lexington, Massachusetts,” prepared for Dinosaur Capital Partners, prepared by Howard Stein Hudson, dated December 4, 2024.

Hancock specifically reviewed and provided comments for the civil plan set, the stormwater report, and the project narrative. Hancock understands the sewer and water systems designs will be reviewed by others.

Hancock used standard engineering practice as a guide in reviewing the stormwater design for the project. Hancock reviewed the application for compliance with the following state manuals and Lexington General Bylaws (LGB):

- Massachusetts Department of Environmental Protection (MassDEP) Stormwater Handbook;
- LGB Ch. 114 Stormwater Management, adopted 3/31/2008;
- LGB Ch. 130 Wetland Protection, amended 5/3/1982, and “Rules Adopted by the Lexington Conservation Commission Pursuant to the Code of the Town of Lexington for Wetland Protection, Chapter 130;”
- LGB Ch. 176 Planning Board Zoning Regulations, Sec.12.9 Utilities, amended 8/30/2017; and
- LGB Ch. 181 Art. VI Stormwater Management Regulations, adopted 9/18/2023.

PROJECT OVERVIEW

The existing development includes a one-story building and a surface parking lot. The Applicant is proposing to demolish the existing structures and construct a 130-unit five-story residential building, a 90-space underground parking garage, and a 43-space parking lot. The property is located within the Manufacturing (CM) zoning district and within the Village High-Rise Overlay (VHO) district.

LGB CH. 181, ART. VI STORMWATER MANAGEMENT REGULATIONS

§181-71.A Applicability

The Stormwater Management Regulations apply to this project, as proposed activity results in a land disturbance of greater than one acre of land.

§181-71.B Project Classification

The proposed project is classified as an **Above Threshold Project**, as the proposed activity results in a land disturbance of greater than one acre of land.

§181-72.A Permit Required

The proposed project requires Site Plan Review. The stormwater management permit shall be consolidated into the Planning Board's site plan review approval. No separate stormwater permit is required.

§181-72.B Stormwater Management Permit Application

Subsection (1)(i) states that Above Threshold Projects subject to the NPDES Construction General Permit shall submit a copy of the Notice of Intent and a copy of receipt of EPA Authorization letter with the Application.

- 1. Hancock understands that once a General Contractor is engaged with the Applicant, a copy of the NPDES Construction General Permit with associated documents will be provided. This is an acceptable approach.***

§181-73.A Minimum Performance Standards

Above Threshold Projects shall meet Standards 1 through 10 of MassDEP Stormwater Management Standards AND the Lexington Stormwater Management Regulations. Stricter requirements shall apply.

§181-73.B Additional Design Criteria

Subsection (2)(c) requires rainfall data based on the Northeast Regional Climate Center "Atlas of Precipitation Extremes for the Northeastern United States and Southeastern Canada" with values rounded to the nearest one-tenth of an inch.

- 2. Hancock requests the Applicant provide documentation of the rainfall depths from the source material (excerpts, tables, etc.) and source material for the 1-year 24-hour rainfall data (table from NOAA website is sufficient). Rainfall depths should be rounded to nearest 0.1 inch.***

Subsections (2)(1), (2)(m) & (2)(n) require either retention of runoff volume or removal of Total Suspended Solids and Total Phosphorus.

- 3. The proposed development is expected to reduce phosphorus loads in the post-construction condition by 96% (requirement is 60%) and TSS loads by 96% (requirement is 90%) through a combination of pretreatment and infiltration. This regulation is met.***

Subsection (3) requires the Applicant to determine whether stormwater discharges from the proposed Site will contribute to the impairment of an impaired water body with or without approved total maximum daily load, and to select BMPs that will control the discharge of pollutants identified as causing the impairment.

- 4. *The Site ultimately discharges into the Shawsheen River which has a Bacterial TMDL or the Shawsheen River Basin with a pollutant of Fecal Coliform. The Project has been designed to minimize stormwater discharge and associated pollutants through extensive infiltration practices to meet the intent of the TMDL. This regulation is met.***

§181-74.A Erosion and Sediment Control Design Criteria

There are twenty-two (22) erosion and sediment control performance standards listed in this regulation that must be met.

- 5. *The Civil Notes, Legend, and Abbreviations sheet (Sheet C-000) from the civil plan set includes a comprehensive list of Erosion and Sediment Control Notes. To ensure this regulation is met, Hancock recommends appending the §181-74.A performance standards to the notes.***

§181-75.D Operation and Maintenance Plan

An Operation and Maintenance (O&M) Plan is required at the time of application for all Above-Threshold Projects. The O&M Plan shall be a stand-alone document, submitted as a digital file, preferably .PDF, in addition to paper copy and shall remain on file with the Stormwater Agency.

- 6. *Hancock recommends the entirety of Appendix E (the Long-Term Pollution Prevention Plan and Stormwater Operation and Maintenance Plan as well as the two figures) be provided as a stand-alone document.***

LGB CH. 130, WETLAND PROTECTION & RULES ADOPTED BY LEXINGTON CONSERVATION COMMISSION

SECTION 5. Performance Standards

Per Standard (2), calculated runoff volumes shall be rounded to the nearest 1 cubic foot.

- 7. *Runoff volumes in the stormwater report (Table 4) and the HydroCAD results should be revised from acre-feet to cubic feet.***

Per Standards (2) and (6), proposed work shall not result in an increase in peak runoff rates between the pre-development and post-development conditions, where “pre-development” conditions means “conditions at the time of the proposal *except* for the footprint of the structures... on the property that are to be demolished, removed, or otherwise taken out of service.” These areas are instead considered as “open space in good condition” and are to be hydrologically modeled as such.

- 8. *The existing HydroCAD model assumes the entire property (Subcatchment 1) is grass cover in good condition, with a Hydrologic Soil Group A in agreement with NRCS designations. This is a more conservative approach as the 0.49 acres of woodland could have been considered in a weighted CN calculation resulting in a high CN. This performance standard is met.***

CIVIL PLAN SET COMMENTS

Sheet C-000

- 9. *Under General Notes, revise or remove Notes 13 & 14.***

Sheet C-100

- 10. *Proposed construction entrance is in close proximity to proposed driveway to parking garage, and access to site may be inhibited after building foundation is poured. Show additional or alternative locations for a construction entrance, and/or provide construction sequencing.***
- 11. *Provide dimensions of construction entrance, making sure they agree with Construction Entrance detail notes (Sheet C-700).***
- 12. *Provide additional silt sacks across Hartwell Avenue to minimize sediment transfer from vehicles leaving the site and traveling northeast on Hartwell.***

Sheet C-200

- 13. *Provide location for dumpster pad and/or confirm that trash collection will be internal to the building.***
- 14. *Label the 25' setback as a parking setback. Show dimensions from closest parking stalls to property line to demonstrate compliance.***
- 15. *Review and adjust callout arrows.***

Sheet C-300

- 16. *Add a note at the garage entrance that surface runoff beyond the trench drain sheet flows into the garage and is collected via floor drains, and to refer to Project Plumbing Plans.***

Sheet C-400

- 17. *CB-106 appears unnecessary, as surface grading slopes away from the catch basin. Revise as needed.***
- 18. *Provide a test pit table stating the TP existing grades, GW depths and Estimated Seasonal High Groundwater (ESHGW) elevations. Add ESHGW to the subsurface infiltration system callouts to better demonstrate minimum 4' offset to groundwater.***

19. Add a note at the garage entrance that surface runoff beyond the trench drain sheet flows into the garage and is collected via floor drains, and to refer to Plumbing Plans.

20. Add weir elevations to OCS callouts.

Sheet C-500

21. Add sewer inverts at BLDG SEWER-1 callout and proposed SMH-300. Calculate and callout sewer pipe slope.

22. Running a water line in close proximity to drainage chambers is challenging to install, and utility trenches can easily transfer stormwater to other areas in the site. Look to possibly relocate the water line around the infiltration system closer to the building.

Sheet C-600

23. No action required.

Sheet C-700

24. See C-100 Comment 3.

Sheets C-701 & C-702

25. No action required.

Sheet C-703

26. Check count in Traffic Sign Summary – there appears to be one R7-8P and one R7-8 sign called out on Sheet C-200.

27. Could not find a concrete pad callout on Sheet C-200. Remove detail or update C-200 with dumpster pad (see C-200 Comment 5).

Sheet C-704

28. In detail 12" TRENCH DRAIN DETAIL IN CONCRETE WALK, revise callouts for Notes #3 and #4.

29. In WATER QUALITY STRUCTURE DETAIL, revise peak flow rate for WQS#302 to 0.63 to agree with Link Summary in stormwater report.

Sheet C-705

30. Add inlet and outlet inverts to OUTLET CONTROL STRUCTURE details. Add 2" plugged orifice in weir plate for OCS-1 at an elevation below invert (for potential future maintenance of chambers).

Sheets C-706 & C-707

31. No action required.

**STORMWATER REPORT COMMENTS &
MASSDEP STORMWATER MANAGEMENT STANDARDS**

Standard 1: No new untreated stormwater conveyances to wetland resources area.

32. The proposed development meets this standard. No further action is required.

Standard 2: Stormwater management systems must be designed so that post-development peak discharge rates do not exceed pre-development peak discharge rates.

33. The Applicant provided information that indicates that this standard has been met; however, some changes to the calculations have been recommended. This standard will be reviewed again once the applicable changes have been made.

Standard 3: Annual recharge to groundwater.

34. The impervious area values provided in the calculation do not agree; please correct. Provide a statement that all stormwater runoff from impervious areas reach a recharge device, or provide a capture area adjustment calculation.

35. Revise "Table 11".

36. State how provided recharge volumes were calculated. If taken from HydroCAD, make reference to the appropriate appendix.

Standard 4: For new development, stormwater management systems must be designed to remove 80% of the average annual load (post-development conditions) of Total Suspended Solids (TSS).

37. The proposed development meets this standard. No further action is required.

Standard 5: Stormwater discharges from areas with higher potential pollutant loads require the use of specific stormwater management Best Management Practices (BMP) (see chart on page 1-8). The use of infiltration practices without pretreatment is prohibited.

38. This standard is not applicable. No further action is required.

Standard 6: Stormwater discharges to critical areas must utilize certain stormwater management BMPs approved for critical areas.

39. This standard is not applicable. No further action is required.

Standard 7: Redevelopment of previously developed sites.

40. This standard is not applicable. No further action is required.

Standard 8: Erosion and sediment controls must be implemented to prevent impacts during construction or land disturbance activities.

41. The proposed erosion and sediment control plan meets this standard. No further action is required.

Standard 9: All stormwater management systems must have an O&M Plan to ensure that systems function as designed.

42. Provide the Operation and Maintenance as a stand-alone document.

Standard 10: Prohibition of Illicit Discharges.

43. A signed illicit discharge compliance statement has been provided. No further action is required.

Appendix A, MassDEP Checklist for Stormwater Report

44. If capture area adjustment is required (see Standard 3 comment), then revise Page 4 of checklist as needed.

45. Provide a completed SWPPP, or revise Page 8 of checklist to say that a DRAFT copy of the SWPPP is included and that a completed SWPPP will be submitted before land disturbance begins.

Appendix G

46. Provide NRCS soil map and summary.

Appendix H

47. Remove the first two pages.

SUMMARY

Additional information is required to complete review of this project. Currently, the project as submitted does not provide sufficient information to show that the proposed project meets the requirements of the Lexington Stormwater Regulations.

Hancock recommends the Applicant submit written comments to this letter and provide the additional requested detailed information to the Planning Board for review.

Please do not hesitate to contact our office should you have any questions or concerns.

Regards,
Hancock Associates



Brian G. Geaudreau, PE
Sr. Associate & Project Manager



Robert M. DiBenedetto, PE
Project Engineer



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Abby McCabe, Planning
Director
Meghan McNamara,
Assistant Director
Carolyn Morrison, Planning
Coordinator



To: Lexington Planning Board
From: Meghan McNamara, Assistant Planning Director
Re: Site Plan Review and Special Permit for 7 Hartwell Ave.; Village High-Rise Overlay District
Date: January 24, 2025

Property Information	
Project Address	7 Hartwell Avenue
Parcel ID	Map 84, Lot 81
Permit #	PLAN-24-26
Applicant	Dinosaur Capital Partners LLC
Owner Name	7 Hartwell LLC
Type of Review	Site Plan Review - Village High-Rise Overlay District Special Permit – Relief from Minimum Yards for Parking
Zoning District	CM - Manufacturing & VHO – Village High-Rise Overlay Zoning Districts
Property Size	69,999± SF (or 1.606 Acres)
Site/Environmental Conditions	A triangular shaped parcel, 7 Hartwell Avenue is bounded by Hartwell Avenue to the southeast, Westview Street to the southwest, Westview paper street to the north, and a small portion of Bedford Street to the northeast. Under current conditions, only one curb cut exists off of Westview Street. The site does not contain any protected resource areas under the jurisdiction of the Conservation Commission, nor any buffer zones. The existing site is relatively flat, and along the portion of the property line that is adjacent to the paper street, the slope becomes steep towards the north of the site. A majority of the site sits roughly 15 feet below the wooded paper street. The existing site contains a one-story building and surface parking lot.

Important Dates/Timelines	
Public Meeting	January 30, 2025
Filed with Town Clerk	December 20, 2024
Decision Deadline (150 days)	May 19, 2025

Approval Information	
Relief Requested	Special Permit pursuant to §135-5.1 and 9.4 of the Zoning Bylaw: Parking spaces and drive aisle within 25 feet of street line for parking within the Westview right-of-way out paper road layout

Relief for the Applicant to Consider	§5.1.13(1a)(*) to allow parking spaces that abut a wall to have less than a 12-foot width. It does not appear that this is met for all spaces in the parking garage. Provide dimensions and to confirm.
Action Required at Decision Deadline	The site plan review decision of the Planning Board shall be by a majority vote of the Board and the special permits shall be a two thirds majority. The Project is permitted by-right and site plan review approval is required. The Planning Board shall review and act upon the site plan, requiring such conditions as necessary to satisfy the Review Standards and the Zoning Regulations.
Applicability	Under the provisions of §135-7.5.1, the purpose of the Village Overlay District is to provide family housing, ensuring compliance with MGL c. 40A § 3A, and to promote multi-family housing in areas that are close to public transportation, shopping, and local services, by offering a variety of housing types.
Waivers from Board's Regulations	<ul style="list-style-type: none"> • 12.4.2.9: parking shall not require lifting bicycles off the floor or carrying bicycles up or down stairs. This is not met for the stacked bicycle spaces. Staff support this waiver request because 67% of the long-term bike spaces are at-grade/not in the lift-assist rack. • 12.4.2.7: each bicycle parking spaces shall be at least six feet by two feet. See note above • 12.4.2.5: bicycle parking shall be separated by at least three feet from any motor vehicle parking spaces to minimize possible damage to bicycles and vehicles. This does not appear to be met for exterior bike parking spaces.

Project Summary				
<p>Applicant proposes to demolish the existing single-story commercial building at the site and construct a five-story, 130-rental unit building and a 300 SF café. The underground parking garage contains 90 parking spaces and the surface parking lot contains 43 spaces (2 allocated for the proposed commercial café use). Applicant proposes a new curb cut off of Hartwell Avenue to access the site as an entrance-only drive, and an additional curb cut off of Westview Avenue to access the underground garage, for a total of 3 curb cuts. Site amenities to include a public playground, café plaza, resident interior courtyard, long-term and short-term bike parking spaces, and 4688 SF interior amenity space. The Applicant is providing 20 affordable units (see chart below) limited to households earning 80% of the Area Median Income and eligible for inclusion on the EOHLC Subsidized Housing Inventory. On October 16, 2024 the Applicant met with staff for a Development Review Team (DRT) meeting to review the proposal. DRT notes are in the application portal. The Applicant held a neighborhood meeting on December 3, 2024 and invited all abutters within 300 feet. On January 22, 2024 Planning Board members visited the property for a site visit.</p> <p>Proposed Unit Count: (x/x) = (Market Rate Total/IDU Total)</p>				
Unit-Type	Total Number	Bedroom Count	Avg. Unit Size (SF)	% of Total
Studio	10 (8/2)	10	500	7.7%
Junior 1 Bedroom	18 (15/3)	18	625	13.8%
1 Bedroom	44 (38/6)	44	660	33.8%

1+ Bedroom	18 (15/3)	18	800	13.8%
2 Bedroom	26 (22/4)	52	960	20%
3 Bedroom	14 (12/2)	42	1,065	10.8%

Vehicle and Bicycle Parking:

Vehicle Parking	Amount Required		Provided	Notes
Residential Vehicle Parking	1 car per unit	130	130 (90-garage, 40-surface)	In compliance
Commercial Vehicle Parking ¹	1 per 5 seats, or 1 per 200 SF, whichever is greater	2	3	In Compliance, if café greater than 15 seats or 600 SF will need more parking
Residential Long-Term Bike Parking	1.5 per unit	195	192	See waiver request. Plans note 195 spaces but only 192 can be located.
Residential Short-Term Bike Parking	0.1 per unit	13	14	Confirm detail and dimensions
EV Vehicle Charging ² (Garage)	Min 4%	4	4	In compliance
EV Vehicle Charging ² (Surface Lot)	Min 4%	2	2	In compliance, however they are compact spaces only.

¹- Per 135-7.5.6.2: the parking factor for all other uses shall be the same as in § 5.1.4 (Table of Parking Requirements) for the CB District

²- Per 135-5.1.13(11): the parking lot (garage and surface lot considered separate lots for this purpose) shall be constructed with appropriate conduits and space for transformers and switchgear to allow for future installation of EV charging stations for a minimum 50% of total off-street parking spaces. *Applicant shows the appropriate amount of future-EV spaces for the garage and surface lots.*

Other Memos:

The Planning Board hired a peer review consultant to assist in this review. Peer Review memo from Brian Geaudreau, Hancock Associates, is included and focuses on site stormwater management, sanitary sewer, water supply system and other utility site planning elements. Comments regarding stormwater management and site utilities have been limited in the staff memo for this reason.

In addition to public comment(s), the Planning Board has received memos from the Design Advisory Committee (dated 1/16/25) and the Bicycle Advisory Committee (dated 1/12/25) [provided under separate cover].

A memo is also provided from Liz Rust, RHSO and is under separate cover. Ms. Rust's memo reference a previous iteration of the plans that proposed 135 dwelling units, and staff have requested an updated memo based on current proposal.

Chapter 135 Zoning Bylaw Review

With the information provided, Staff believes the proposed development meets the requirements of Chapter 135, except as noted otherwise below.

Section 4.0 Dimensional Controls

4.3.1 Structures Other than Buildings

Retaining walls are allowed on the property line if less than four feet. Any retaining wall taller than four-feet shall be set back at least equal distance to their height. Are the wall structures to the right of the Hartwell entrance for aesthetics or are those retaining? Please provide top of wall and bottom of wall height of these structural walls, as well as the retaining wall along the café area seating area.

4.3.2 Structures on a Building

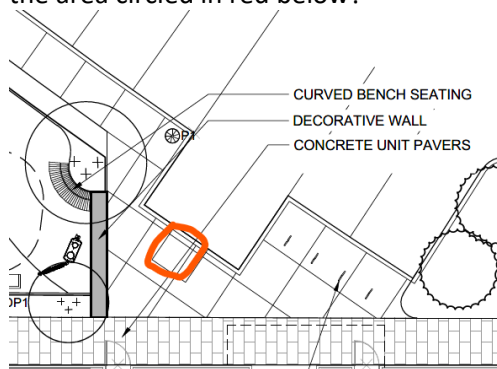
Structures on a building not used for human occupancy can exceed max height of a building if no part of the structure is more than 20 feet higher than the upper elevation of a building and total horizontal coverage does not exceed 25%, not including Solar PV systems. How is the roof accessed for maintenance?

4.5.4 Estimated Seasonal High Groundwater Table (ESHGWT)

The vertical distance between the finished basement floor and any dwelling shall not be less than two (2) feet above ESHGWT. Confirm the elevation of the finished basement floor, and what is the ESHGWT elevation?

Section 5.0 General Regulations

- No loading bay required for the proposed use/square footage (5.1.5), however the Applicant proposes a loading zone near the playground, presumably for resident move-in/out and waste removal services. Provide signage and label this area as a loading zone on the planset. What is the area circled in red below?

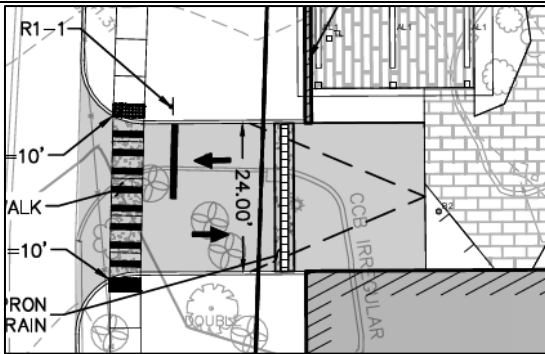


- No short-term bicycle parking required for the proposed commercial use (5.1.8)
- The number of driveways permitted per street line is two. Applicant proposed two driveways off of Westview Avenue and one driveway off of Hartwell Avenue (5.1.10.2)

5.1.11 Minimum Yards for Parking

All paved parts of parking spaces, driveways and maneuvering aisles shall be set back 25 feet from a street line and 5 feet from the wall of a principal building in the CM District. Applicant has submitted revised plans that eliminate parking spaces within 25 feet of Westview Street. Upon staff's recommendation, the Applicant proposes a drive aisle and parking spaces within 25-feet of the paper street portion of Westview Street.

The garage entrance driveway is within 5-feet of the building wall. The width of this 2-way entrance is 24 feet and only 18 feet is required (5.1.13.1.d). The plans show pavement up to the building wall, however the drive aisle width does not need to be considered that close.



5.1.13 Design Standards

- The required width of a maneuvering aisle for standards size parking spaces is 22 feet. 20 ft. is shown between the 5 parking spaces when entering from the northerly Westview entrance. Please widen by 2 ft. is required.
- “In parking lots containing more than 20 spaces, not more than 33% of such spaces may be designed for use by compact cars” [5.1.13\(2\)](#). Six (6) compact spaces are proposed on the surface lot (14%) and 14 compact spaces are proposed in the garage (16%). This complies.
- Any parking space with an angle of parking at 90-degrees, where one or both of the long sides of the space abut a wall or similar obstruction, the width shall be 12 feet. It does not appear that this requirement is met for all spaces in the parking garage. Please provide setback measurements for spaces that abut a wall.
- “The parking lot shall be constructed with appropriate conduits and space for transformers and switchgear to allow for future installation of EV charging stations for a minimum of 50% of the total off-street parking spaces...” [5.1.13\(11\)](#). Show on plans where the transformer and switchgear locations will be.
- No loading bay is required for this development, however staff request a turning diagram be provided showing a large box truck and/or trash truck entering and exiting this parking space. The Zoning Bylaw states that all required loading bays have minimum dimension of 30 ft by 12 feet, and the maneuvering aisle be equal to it’s length. Since the loading bay is not required, these dimensions may not be applicable.
- Show snow storage areas on plans.

5.2 Signs

- A preliminary signage package was submitted. Applicant proposes monument signage near the Hartwell Ave entrance. No lighting is proposed. Signage package refers to landscape plans for signage height. Landscape plans refer to civil plans for height and “plans” for length. Please provide in one document all plans related to building signage. Staff cannot locate the actual dimensions of the sign.
- Café signage to be provided separately.

5.3 Landscaping, Transition and Screening (see 7.5.9)

5.4 Outdoor Lighting

- Provide a photometrics plan showing the intensity of illumination at ground level expressed in footcandles. Staff are unable to confirm compliance with 5.4 until this is provided.

- Outdoor lighting is not allowed between the hours on 11 pm and 6 am; except for security purposes provided that the average light on surfaces is no greater than 0.5 foot-candles. Please show which lights will be on overnight.
- All proposed lighting has a Color Rendering Index (CRI) greater than 50. This complies.

Section 7.0 Special District Regulations

7.5.4 Permitted Uses

The Town of Lexington Zoning Bylaw and Zoning Map were amended at the 2023 Annual Town Meeting to include the Village and Multi-Family Overlay District. The project site is located entirely within this district, specifically the Village High-Rise Overlay (VHO) District.

7.5.5 Dimensional Controls

Minimum Required Front Yard (Hartwell Ave): “the lesser of that required in the underlying zoning district or 15 feet”. No minimum front yard is required in the CM District, therefore the setback is zero feet.

- “In the case of...uses located in the CM or CRO Districts, the minimum front yard facing all streets shall be the same as that for the frontage street.” (Table 2, Schedule of Dimensional Controls). Therefore, the setback on all sides of the property is zero feet.

Maximum Height:

The maximum height of buildings in the VHO District is 70 feet and 68 feet is proposed. The number of stories is not restricted. Applicant to provide the Average Natural Grade form and Height Calculation sheet of the proposed building for the Building Commissioner to confirm height compliance. This will also be reviewed and confirmed prior to issuance of a building permit.

7.5.9 Transition Areas

- No Transition Area is required

7.5.12 Inclusionary Housing

“In any development containing fourteen (14) or more dwelling units, at least 15% of the dwelling units shall be Inclusionary Dwelling Units (IDU) with household income limited to 80% of the Area Median Income and eligible for inclusion on the EOHLC’s Subsidized Housing Inventory” [7.5.12\(1\)](#).

- The Applicant is required to provide 19 units and 20 are proposed. Ten-percent of the market-rate, and 10% of the IDU’s are three bedrooms, per EOHLC’s recommendation.
- Applicant proposes that occupants of the IDU’s will pay 60% of the market-rate pricing for parking. Staff appreciate this!

Staff believes the proposed development meets the requirements and/or expectations of Chapter 176, except as noted otherwise below.

5.0 Submission Materials

- Reupload the civil planset with dates on each sheet. The civil plans do not have a date.
- Revise civil details to remove the use of straw waddles as proposed erosion control. Per Lexington Stormwater standards, only compost-filled silt socks are to be used.
- Once the building foundation is in, it would seem that the construction access is not usable. Recommend the construction entrance be located to the northwest corner of the site, where the proposed second entrance will be.
- The limit of work, construction entrance fencing and erosion control is proposed within the public ROW of Westview Street and Hartwell Avenue. Applicant will be required to seek approval from Select Board for work within public right of way.
- Show limit of work and erosion control on all civil plans.
- Show location of test pits and results. This can be shown on a separate Site Analysis Map if preferred. Review details of 176-5.2.2 'Site Analysis Map'.
- Though typically required on the Existing Conditions plan, the Applicant provides a list of existing tree species to be removed on the Tree Mitigation Plan. Because a majority of trees will be removed from the site, staff find this acceptable to show on the separate plan.
- Note #13 under General Notes on sheet C-000 says "Elevations Refer to XXXX". Please correct.
- Note #14 shall be updated with the correct Conservation Commission filing number once approval is received for the proposed drainage connection.
- What is the clearance height of the garage doors and within the garage?
- Will there be a backup generator?

Section 12.0 Site Plan Review Design Regulations

12.3. Construction Standards

- Adjust the erosion control line along the northern property line so that it is outside the critical root zone of the protected trees within the Hartwell Ave paper street. These protected trees are greyed out on the site demo plan. Label these trees as protected on the site demo plan and it is not obvious on the site demo plan that these are to remain post construction. Priority shall be given to preserving existing stands of trees at the site perimeter.
- Show location of construction staging areas – these shall not be located near buildings, porous surfaces, mature or specimen trees, and sensitive areas.
- Show location of employee parking during construction.
- Can you estimate how much fill will be removed from the site? How much will be brought to the site?

- Add note per §176-12.3.11 (2) that erosion control and best management practice installations shall be checked before the start of construction and at the end of construction daily. Such controls when damaged shall be repaired within 24 hour”

12.4 Access; Parking; Transportation

12.4.1 Site Access

- Access to the site from a public way shall be clearly visible and accessible. Please revise plans to remove the set of stairs that encroach onto the ROW.
- Applicant proposes a new sidewalk and bus shelter within the public ROW. Applicant shall receive approval from Select Board prior to construction. Staff support this request to provide connectivity around the building and support public transit.

12.4.2 Bicycle and Other Device Parking

- Short-term bicycle racks shall be at least 2 feet away from any wall or other obstruction. Confirm that the 14 short-term bike spaces meet this requirement.
- Each bicycle parking space shall be at least 6-feet by 2-feet.
- Applicant has provided a detail for the bike-rack on sheet L-401. Is this the same rack that is proposed in the interior bike storage room?
- 20 E-bike charging spaces are proposed in the long-term storage room.
- Provide a detail for the lift assist. Any available room for larger sized parking, such as for cargo bikes and strollers?

12.4.3 Interior Drives, Driveways, and Roads

- Thank you for proposing one of the exterior ADA parking spaces as a future-EV space. The requirements for accessible EV charging spaces would likely interfere with the proposed walkway around the building. Since not all EV’s have charging ports at the same location, a larger vehicle space may be needed for accessible maneuverability.
- Confirm that all pedestrianways around the building are at least 4-feet in width.

12.5 Building Planning and Design

12.5.1 General

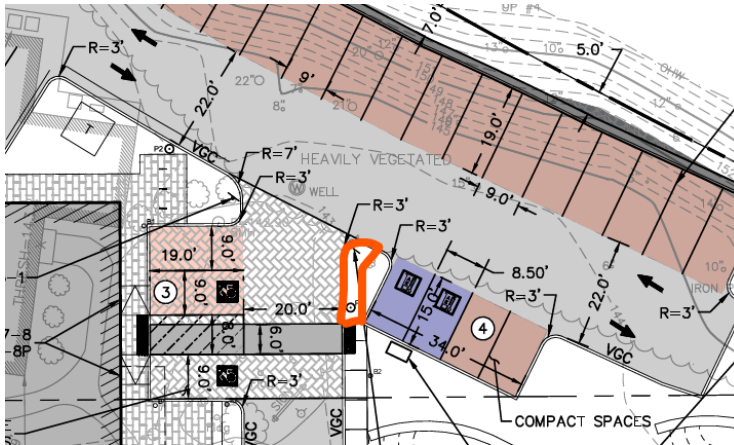
- “Structures and canopies should be designed for solar and other energy systems”. Applicant shows potential areas for solar on the building’s rooftop.

12.6 Landscaping

- All proposed tree species are sourced from the Lexington Preferred Plant Lists. The proposed Honey Locust (*Gleditsia triacanthos*) is non-native. Please replace with a native species.
- Projects that involved disturbance of more than on-half acre of land should be designed to be certifiable at the gold level under SITES v2, as amended. The applicants scorecard indicates 100 estimated points, and gold level is 100 points. Why does the applicant not intend to gain points from protecting air quality during construction?
- Trees shall be planted along the frontage and spaces so that the trees can grow. Tree spacing shall be approximately 30 feet. Applicant proposes numerous 4-inch caliper deciduous trees along the frontage of Hartwell Avenue and Westview Street. The proposed placement of the

trees along Hartwell Avenue are in the current anticipated location of a grass strip between a shared side path and roadway buffer. Staff support this location.

- The residential amenity space and required play area (applicable for this project) shall be at least 24 square feet per dwelling unit. Applicant proposes 4,688 SF of indoor amenity space. What is the estimated square footage of the resident courtyard and playground area?
- Provide curb extensions where pedestrians are required to cross driveways and parking aisles. Can the proposed sidewalk be extended down the entire length of the one-way entrance drive?



12.7 Signage and Wayfinding

- Wayfinding signs are not provided. Provide adequate signage to inform and direct visitors around the site. Provide proposed signage for the café-only parking spaces, one-way entrance on Hartwell Avenue, loading zone, no-parking at the eastern corner of the site, resident-only garage, etc.
- Signage shall be provided to guide bicyclists through the site.

12.8 Outdoor Lighting (and 5.4 of Zoning Bylaw)

- Applicant to provide a photometrics plan showing foot-candle at ground level. No light spill over any property line is allowed. Once provided, staff will review for compliance.

12.9.5 Drainage and Stormwater Management.

- Strive to replicate natural hydrologic conditions and manage precipitation on-site by exceeding the LID and conservation design requirements. Are there any LID design elements incorporated?
- All basement floors and slabs shall be at least two feet above the estimated seasonal high groundwater table. Confirm that this is met.
- Use stormwater harvesting systems, such as cisterns and ponds, for plant irrigation. Has this been considered?

Town Staff Comments

Planning Staff Comments:

- The peer reviewer will be a part of both the ConComm and Planning Board proceedings.
- Applicant will be responsible for any police details needed during construction and for re-routing pedestrians.
- Will there be reserved parking spaces for the Café?
- Is the PSIS at Hartwell Ave entrances setback 5 feet?
- Ensure setbacks are measured to furthest projecting point on building (i.e. balconies)
- Provide Height Calculation Form, ANG Form, and a site plan showing where the spot grade elevations were taken.
- Café sign will have to comply with zoning requirements
- Will fill be brought in? If so, written notice to the Planning Office, DPW and Conservation Department that includes the name of the company, location or fill origin and certification that the fill is clean.
- As a condition of approval, and to be included in DEED/HOA/lease documents, that all dwelling units, including IDU, are prohibited from being used as short-term rentals (6.10.3(2)).
- Appreciate the detailed renderings of the proposed building.
- Compile all civil plans into one set, including landscape plans. Landscape plans need to be updated to reflect latest changes to the parking.
- No work can be done until the limit-of-work line, erosion controls and tree protection are installed and the approved by the Planning Office. This will be clear in the decision and should be clear on the plans.
- Would love to see a sidewalk or protected bike lane or pedestrian walking area along Westview. Would the applicant considering contributing to such a fund?

Building and Zoning:

- The State Energy code requires that 20 % of all parking be EV spaces, of which, 2% should be ADA/Accessible EV spaces, one of which one should be Van Accessible
- There should be a minimum of 7 ADA/AAB Accessible dwelling units (AAB Group 2), with at least one accessible parking space per accessible unit
- All other units must meet Group 1 per Mass Building Code , Architectural Access Board
- The exterior lot should have 2 Accessible parking spaces, one of which is Van Accessible
- There are stairs from Hartwell sidewalk to outdoor patio, is the adjacent sidewalk accessible, and are there curb cuts provided at driveways along Hartwell and Westview
- There is a set of stairs to the building first floor on Westview/Hartwell corner, without an accessible route, is this an entry or exit requiring Accessibility?
- Will there be rooftop mechanical equipment, if so, will it be screened?
- Insure outdoor amenities area , kitchen, BBQ, walkways, benches, playground equipment and all surfaces are ADA/AAB compliant
- All retaining walls must be set back a distance equal to the height of the wall
- Are there any fences planned to be installed?
- A site plan locating points to be used on the elevation/height worksheet should be submitted, along with the zoning /height form, to confirm average natural and building height. Typically,

use points at perimeter of building to insure an accurate average, always use garage floor and main entry floor elevations, and at the bottom and top of any wall, and at a straight line grade change. Email me the proposed spot locations for review and we can work on that together.

- Will you be using the 10th edition of the State Building Code? Its effective for permit applications starting July 1, 2025.
- Would the applicant be amenable to pay for a third party consultant to assist the Building Inspectors for plan review of ADA/AAB/FHA code requirements, and to perform Inspections?
- There should be a direct entry from the exterior for access to fire sprinkler roof and Main Electrical room, with signage per code, for emergency response from fire department.

Transportation:

- We request the developer commit to include a welcome packet with information about MBTA, The Rev shuttle, Lexpress and the Minuteman Bikeway with each new lease and that this information be kept up to date and available in the common space.
- I appreciate that this applicant is unbundling parking from rental unit fee, is not providing more parking than required, and has reviewed and become familiar with transit options and will be installing a bus shelter. Please note we have a preferred design for bus shelters. We can share that with the developer and/or Planning staff/Planning board. While there will be smaller bikes for youth, there may also be larger cargo bikes and bikes with trailers, so having a mix of sizes and an ability to have more at grade would be good. Exact location would need to be confirmed prior to installation.

Fire:

- Please confirm that the infiltration system can support the Town heaviest fire truck. This will need to be confirmed before a building permit is issued.
- Show the centerline of Hartwell Avenue and Westview Avenue on the Emergency Vehicle Access Plan (Sheet C-600). Does the fire truck require maneuvering into the opposite lane of the roadway to maneuver around the site?
- The fire truck will not exit the site onto Hartwell Avenue because it is a one-way entrance. Show turnaround diagram of truck entering and exiting Westview Avenue



- Bike room with electric charging stations shall have access to the outside and no interior access.

Health:

- While developing the site, must have a Pest Management and Dust Control Plan in place.
- Applicant will be required to submit necessary Health permits for the proposed café area.

Conservation:

- There is a proposed municipal separate storm sewer system connection from this site's proposed stormwater management system. At a minimum, a Request for Determination of Applicability (RDA) filing is required to be submitted to the Conservation Commission for review simultaneously with the Planning Board process. Will need to be reviewed for compliance with Section 5(2) and 5(6) by the peer reviewer.
- It is recommended that site plan review remain open for the duration of the Conservation Commission's review and approval. The Applicant has filed an RDA with the Conservation Commission's February 3 meeting.

Sustainability & Resilience:

Staff appreciate the careful adherence to the specialized stretch code and Lexington's fossil fuel free bylaw. Thank you for including compost bins in the trash room. Would be great to see solar as part of the completed project.

Engineering:

Applicant has provided Engineering Dept. with proposed water and sewer calculations. Engineering Staff are reviewing and coordinating a capacity flow review with outside consultants. Applicant will be responsible for any service upgrades necessary for connection, though Engineering Staff don't have immediate concerns for the Hartwell Area.

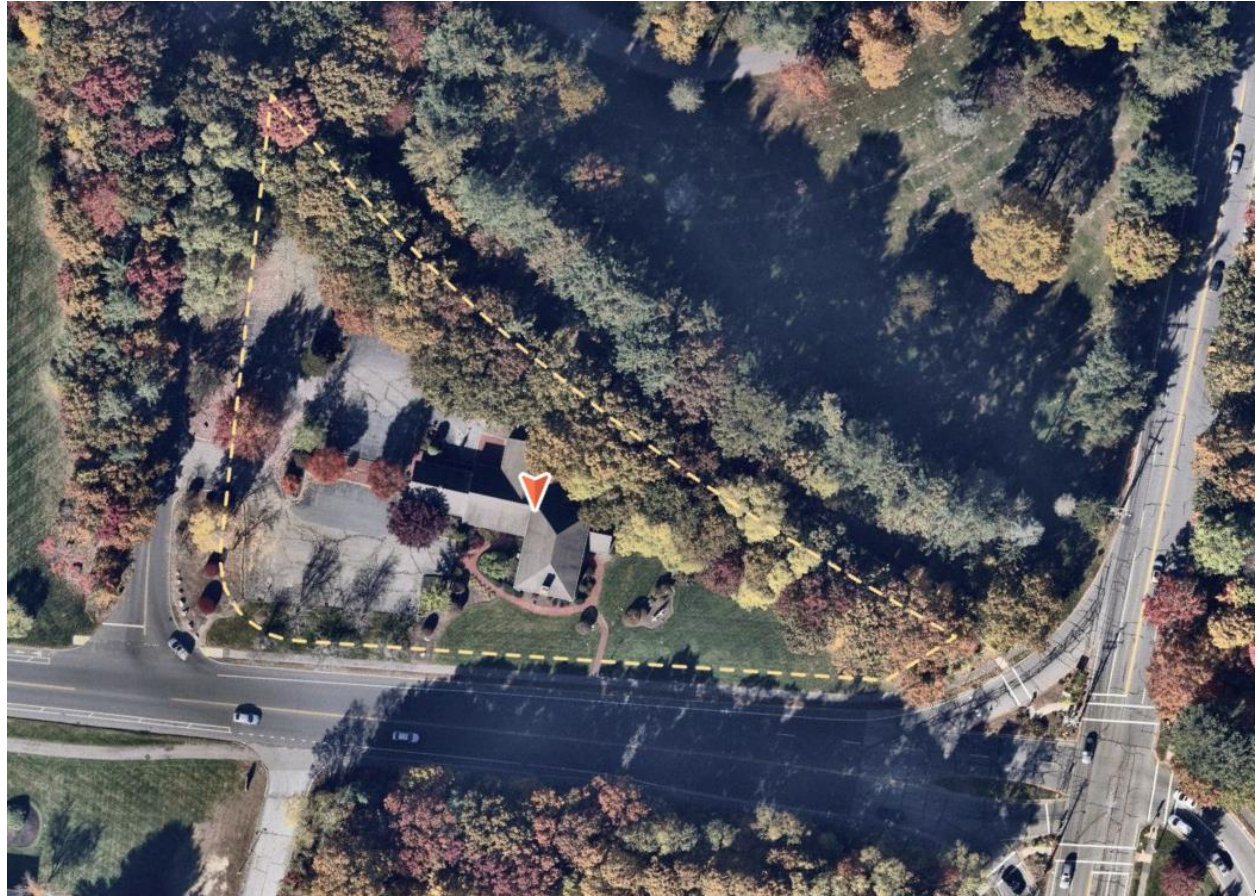
Assessing:

- Addressing for units will need to be determined by Emergency Services and Engineering before any building permits are issued.

Tree Bylaw:

- Please confirm the total number of trees that are to be removed off-site. Staff count 19 trees to be removed within the right of way. The table on the Tree Mitigation Plan totals 15 trees.
- Applicant proposes removal of 50 trees within the setback area, resulting in a required 734-inches DBH required for mitigation. 760 caliper inches of trees are proposed to be planted.
- What is the total DBH of mitigation plantings proposed on the site, and within the right of way?
- Please label the two dead trees proposed for removal. It is difficult to read the tree tag number, especially if on a hard copy of the plans.
- Per the Tree Bylaw, planting may occur in the Town right-of-way with permission of the Tree Warden. It shall be noted that a significant amount of mitigation planting is proposed not on the subject property, though staff appreciate the trees proximity to the public pathways.
- Please review the requirements for removal of a hazardous and/or dead protected tree under the recently adopted Tree Management Manual.

2024 Aerial View and photographs from January 22, 2025 Site Visit





AGENDA ITEM SUMMARY

LEXINGTON PLANNING BOARD

AGENDA ITEM TITLE:

Board Discussion - Zoning Amendment Relative to Bicycle Parking

PRESENTER:

Board Discussion

ITEM NUMBER:

SUMMARY:

Sec. 5.1.8: Request to postpone amendment relative to §5.1.8 bicycle parking.

Staff requests this zoning amendment be postponed to allow more time for a more comprehensive review with the Bicycle Advisory Committee.

SUGGESTED MOTION:

FOLLOW-UP:

DATE AND APPROXIMATE TIME ON AGENDA:

1/30/2025

ATTACHMENTS:

Description	Type
□ Draft Bike Parking Zoning Motion	Exhibit

Town of Lexington
Motion
2025 Annual Town Meeting

ARTICLE AMEND ZONING BYLAW – BICYCLE PARKING

MOTION:

That the Zoning Bylaw, Chapter 135 of the Code of Town of Lexington, be amended as follows:

1. Replace §135-5.1.8 with the following:

5.1.8 Bicycle Parking Facilities.

1. Long-term bicycle parking for employees and residents.
 - a. Where §5.1.4 requires 9 or more motor vehicle parking spaces on a lot for office, manufacturing, research or laboratory uses, at least 1 long-term bicycle parking space shall be provided. One additional long-term bicycle parking space shall be provided for each additional 30 required motor vehicle parking spaces.
 - b. For multi-family housing, at least 1.5 long-term bicycle parking spaces per dwelling unit shall be provided.
 - c. Long-term bicycle parking shall be protected from the elements and in a secure location.
2. Short-term bicycle parking for customers and visitors.
 - a. Where §5.1.4 requires 9 or more motor vehicle parking spaces on a lot for office, manufacturing, research or laboratory uses, at least 1 short-term bicycle parking space shall be provided. One additional short-term bicycle parking space shall be provided for each additional 30 required motor vehicle parking spaces.
 - b. Where §5.1.4 requires 9 or more motor vehicle parking spaces on a lot for other nonresidential uses, at least 2 short-term bicycle parking spaces shall be provided.
 - c. For multi-family housing, at least 0.1 short-term bicycle parking spaces per dwelling unit shall be provided.
 - d. Short-term bicycle parking spaces shall be visible and convenient to a building entrance, be visible (where possible) from a public way, and be at least two feet from any wall or other obstruction.
3. Design.
 - a. Bicycle parking shall not cause obstruction of pedestrian or motor vehicle traffic.
 - b. Bicycle parking shall be situated in such a way that normal snow removal activities and snow storage do not impact the bicycle parking facility.
 - c. Each bicycle parking space shall be sufficient to accommodate a bicycle six feet in length and two feet in width.
 - d. Bicycle parking spaces shall include apparatus to which the frame and wheel of a parked bicycle may be attached; installed in a visible location to deter vandalism and theft; and permanently mounted to the ground or to a building or other immovable structure. Inverted-U-frame or other racks that support the bicycle at two or more points above the center of gravity are required.
 - e. Where there are multiple rows of bicycle parking there shall be a pedestrian aisle with clearance of a minimum of five feet between bicycle racks.
4. Site Plan Review. For projects requiring site plan review, the Planning Board may waive any provisions of this §5.1.8.

Town of Lexington
Motion
2025 Annual Town Meeting

(1/15/2025)

AGENDA ITEM SUMMARY

LEXINGTON PLANNING BOARD

AGENDA ITEM TITLE:

Board Administration & Other Business Items

PRESENTER:

**ITEM
NUMBER:**

SUMMARY:

Board Member & Staff Updates:
Review of Draft Meeting Minutes: 1/15/25
Review spring meeting schedule.
Upcoming Meetings: 2/12, 2/26, 3/5, 3/12, and Thur 3/27.

SUGGESTED MOTION:

FOLLOW-UP:

DATE AND APPROXIMATE TIME ON AGENDA:

1/30/2025

AGENDA ITEM SUMMARY

LEXINGTON PLANNING BOARD

AGENDA ITEM TITLE:

The meeting will continue until all items are finished. The estimated adjournment time is 10:30 pm

PRESENTER:

ITEM NUMBER:

SUMMARY:

SUGGESTED MOTION:

FOLLOW-UP:

DATE AND APPROXIMATE TIME ON AGENDA:

1/30/2025

AGENDA ITEM SUMMARY

LEXINGTON PLANNING BOARD

AGENDA ITEM TITLE:

Members of the public can attend the meeting from their computer or tablet by clicking on the following link at the time of the meeting

PRESENTER:

ITEM NUMBER:

SUMMARY:

SUGGESTED MOTION:

FOLLOW-UP:

DATE AND APPROXIMATE TIME ON AGENDA:

1/30/2025